

ORDINANCE NO. 2023-07

AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF AMERICAN CANYON AMENDING THE WATSON RANCH SPECIFIC PLAN (PL22-0023)

WHEREAS, pursuant to Section 65300 of the State Planning and Zoning Law (Gov. Code § 65000 *et seq.*), the City of American Canyon has adopted a General Plan to provide comprehensive long-range planning and a blueprint of the city's future form, including land use and circulation maps that specify the roadway network and the distribution of types and intensities of land; and

WHEREAS, Section 65358 of the State Planning and Zoning Law provides that the City Council may amend all or part of the General Plan if it deems the amendment to be in the public interest; and

WHEREAS, the Watson Ranch Specific Plan (WRSP) is a comprehensive planning document that encompasses 309-acres and will contribute significantly to the identity of American Canyon with its unique draw of new residential neighborhoods and the Napa Valley Ruins & Gardens (NVRG); and

WHEREAS, the WRSP area is located within north of Vintage Ranch; east of the Union Pacific Rail Line; west of the City limits, and generally south of Watson Lane; and

WHEREAS, on October 16, 2018, the City Council of the City of American Canyon amended the General Plan to recognize and ensure consistency with and between the City of American Canyon General Plan and the Watson Ranch Specific Plan; and

WHEREAS, on October 16, 2018, the City Council certified the Watson Ranch Specific Plan Draft and Final Environmental Impact Report (collectively, the "WRSP Final EIR") pursuant to the California Environmental Quality Act (Pub. Res. Code § 21000 *et seq.*, ("CEQA")); and

WHEREAS, on November 6, 2018, the City Council of the City of American Canyon adopted the Watson Ranch Specific Plan by Ordinance pursuant to Municipal Code Chapter 19.12.020, establishing it as the regulating document for the development of the Watson Ranch Specific Plan site; and

WHEREAS, the Watson Ranch Specific Plan (WRSP) is a detailed planning document which provides a land use and policy framework for future development of approximately 309 acres within the City of American Canyon; and

WHEREAS, as a policy framework for development of 309 acres, the WRSP by necessity will require amendment from time-to-time to provide appropriate modifications to address current needs and improved ideas; and

WHEREAS, on June 4, 2019, the City Council of the City of American Canyon amended the General Plan to accommodate technical revisions to the Watson Ranch Specific Plan (Resolution 2019-40); and

WHEREAS, on June 18, 2019, the City Council of the City of American Canyon approved the first amendment to the WRSP (Ordinance 2019-05); and

WHEREAS, subsequent to June 18, 2019, the City of American Canyon has approved numerous implementing discretionary applications that have resulted in a Development Agreement; Large Lot Vesting Tentative Map; Street and Park Dedications; Lemos Pointe Apartments, Artisan and Harvest single-family homes; Tentative and Final Maps for single family subdivisions; the NVRG Hotel and Condominiums; two NVRG Amphitheaters; and approval of ministerial construction permits; and

WHEREAS, without changing the approved residential and commercial development intensity, the applicant, AC1-LLC, proposed a General Plan Amendment and Watson Ranch Specific Plan Amendment to implement improved ideas for a more efficient roadway circulation, park locations, public school needs, and clarification of narrative language and policy descriptions throughout the Specific Plan document (PL22-0023); and

WHEREAS, on May 25, 2023, the City of American Canyon Planning Commission conducted a public hearing and unanimously recommended approval of the Watson Ranch General Plan Amendment and Watson Ranch Specific Plan Amendment; and

WHEREAS, on August 15, 2023, the City of American Canyon City Council conducted a public hearing and approved the Watson Ranch Specific Plan General Plan Amendment; and

WHEREAS, on August 15, 2023, the City of American Canyon City Council conducted a public hearing, at which time all those in attendance were given the opportunity to speak on this Watson Ranch Specific Plan Amendment.

NOW, THEREFORE THE CITY COUNCIL OF THE CITY OF AMERICAN CANYON DOES HEREBY ORDAIN AS FOLLOWS:

SECTION 1. Findings to Approve the Specific Plan Amendment in accordance with American Canyon Municipal Code Section 19.17.080

- A. The distribution, location and extent of land uses, including open space, as depicted in the specific plan is consistent with the general plan.

The Watson Ranch Specific Plan sets forth the land uses, residential densities, non-residential intensities, permitted and conditional uses, development standards, and design guidelines for the WRSP Area and further implements the Policies set forth for the Town Center land use designation. The Watson Ranch Specific Plan is consistent with the General Plan. This Amendment has a corresponding amendment to the General Plan and is therefore consistent.

- B. The specific plan provides for public infrastructure and services needed to support the land uses described in the plan, including adequate distribution, location, extent and intensity of transportation, sewage, water, drainage, solid waste disposal, energy, parks, community facilities and other essential facilities.

The Specific Plan does set forth the design for roadways, water, wastewater, recycled water, storm drainage systems, parks and other community facilities necessary to support the land

uses described in the plan. Specifically, Chapters 4, 5, 7 and 8 of the WRSP address these facilities. This Amendment is consistent with the provision of these facilities.

- C. The standards and development criteria, including requirements for resource utilization, will ensure that development proceeds in an orderly fashion and maintains a high level of quality.

Chapter 6 of the WRSP sets forth development standards which are supplemented by Design Guidelines in Appendix A. The development standards and design guidelines together promote efficient resource utilization, best practices, and sustainability to ensure a high level of quality. Further Chapter 9, Implementation, sets forth the phasing program to ensure the development proceeds in an orderly fashion. This Amendment does not alter the standards and development criteria set forth in the Specific Plan and is therefore consistent.

- D. The specific plan contains implementation measures, including financing programs, to ensure that development is supported by adequate infrastructure as it occurs.

WRSP Chapter 9, Implementation, sets forth the financing principles and policies, identifies different financing mechanisms, and sets forth phasing of supporting infrastructure. This Amendment supports the provisions of the Development Agreement which, consistent with the Specific plan, provides further clarity on financing and phasing of infrastructure. Therefore, this Amendment does not materially alter the implementation plan set forth in the Specific Plan and is therefore consistent.

- E. The site is suitable for the type and intensity of development proposed.

As determined through the initial existing conditions analysis and the preparation of the WRSP Final EIR, the WRSP site is suitable for the type and intensity of the proposed development associated with this Amendment. The Amendment does not alter the type and intensity of development proposal and is therefore consistent.

- F. The flexibility in development standards afforded by the specific plan process has resulted in a project providing more superior design and amenities than would occur under more traditional zoning practices, and the project provides clear and substantial benefit to the city.

The WRSP Project implements the General Plan for the Town Center land use designation, consistent with Objective 1.19 of the General Plan: "Provide for the development of a Town Center that physically and functionally serves as the symbolic and identifiable focus of community activities and events for the City of American Canyon and which is a regional destination within Napa Valley." General Plan Policy 1.19.7 designated that the land uses in this area be developed pursuant to one or more specific plans. The WRSP includes Development Standards and Design Guidelines that promote diverse neighborhood design and integrated housing types. It further provides for the creation of a "Town Center" through the renovation of the cement factory ruins while preserving the unique character of this site feature. This Amendment does not alter the flexibility in development standards, the superior design or amenities of the Specific Plan and is therefore consistent.

SECTION 2: Based on the findings described above, the City Council of the City of American Canyon hereby amends the Watson Ranch Specific Plan as depicted in Exhibit A to this Ordinance.

SECTION 3: CEQA FINDINGS: Regarding Environmental Review pursuant to the California Environmental Quality Act (CEQA)

1. This Specific Plan Amendment does not change the total number of dwelling units and commercial square footage established by the WRSP. Therefore, the environmental impacts of the WRSP Project which included the Specific Plan were adequately considered in the certified Final EIR for the Watson Ranch Specific Plan Project (SCN. 2015022030) which includes findings, a Statement of Overriding Considerations for the Project, and a Mitigation Monitoring and Reporting Program.
2. Approval of the Specific Plan Amendment based on the Final EIR, the Statement of Overriding Considerations for the Project, and the Mitigation Monitoring and Reporting Program complies with CEQA.

SECTION 4. EFFECTIVE DATE. This ordinance shall become effective effect 30 days after its final passage pursuant to Government Code section 36937.

SECTION 5. SEVERABILITY. If any section, sentence, clause or phrase of this Ordinance is for any reason held to be invalid or unconstitutional by a decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of this Ordinance. The City Council hereby declares that it would have passed this Ordinance and adopted this Ordinance and each section, sentence, clause or phrase thereof, irrespective of the fact that one or more sections, subsections, sentences, clauses or phrases be declared invalid or unconstitutional.

SECTION 6. CUSTODIAN OF RECORDS. The documents and materials that constitute the record of proceedings on which this Ordinance is based are located at the City Clerk's office located at 4831 Broadway, Suite 201, American Canyon, CA 94503. The custodian of these records is the City Clerk.

The foregoing Ordinance was introduced at a regular meeting of the City Council of the City of American Canyon, State of California, held on the 15TH day of August, 2023 by the following vote:

AYES:	Councilmembers Aboudamous, Joseph, Vice Mayor Washington, and Mayor Garcia
NOES:	None
ABSTAIN:	None
ABSENT:	Councilmember Oro


The foregoing Ordinance was adopted at a regular meeting of the City Council of the City of American Canyon, State of California, held on the 5TH day of September, 2023, by the following vote:

AYES: Councilmembers Aboudamous, Joseph, Vice Mayor Washington, and Mayor Garcia
NOES: None
ABSTAIN: None
ABSENT: Councilmember Oro

ATTEST:



Taresa Geilfuss, CMC City Clerk



Leon Garcia, Mayor
APPROVED AS TO FORM:



William D. Ross, City Attorney

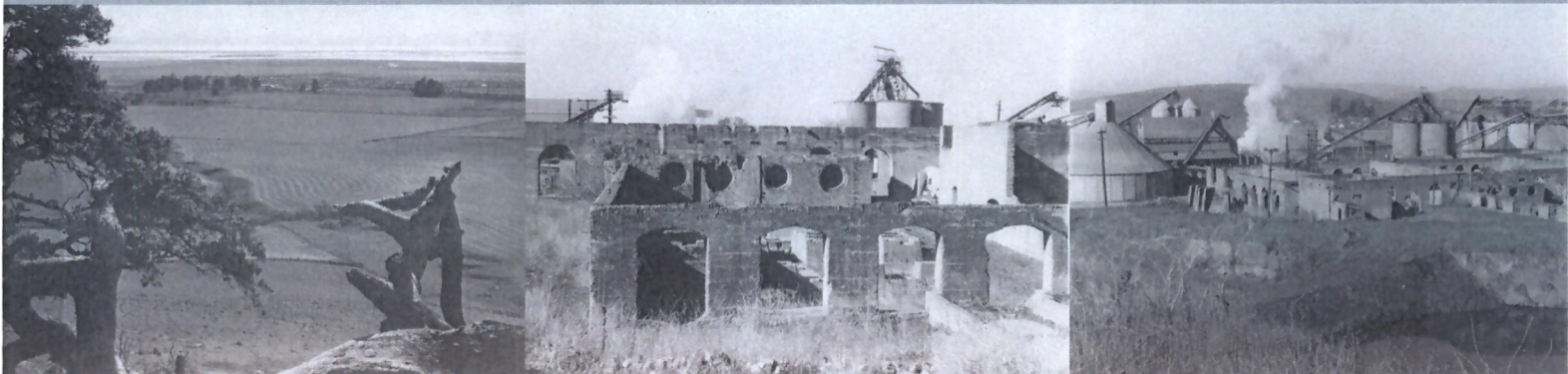
Exhibits

A. Revised Watson Ranch Specific Plan

EXHIBIT A

WATSON RANCH SPECIFIC PLAN

**SPECIFIC PLAN AMENDMENT
APRIL 2023**



ACKNOWLEDGEMENTS

City of American Canyon

City Council

Leon Garcia, Mayor

Pierre Washington, Council Member

Mariam Aboudamous, Council Member

Mark Joseph, Council Member

David Oro, Council Member

Kenneth Leary, Council Member*

Planning Commission

Crystal Dispo Mallare, Chair

Eric Altman, Commissioner

Brando Cruz, Commissioner

Andrew Goff, Commissioner

David Mohammed, Commissioner

Tyrone Navarro, Commissioner*

Tammy Wong, Commissioner*

Bernie Zipay, Commissioner*

Parks and Community Services Commission

Clarence Mamaril, Chair

Cathy Margolati, Commissioner

David Garcia, Commissioner

Charlie Plummer, Commissioner

Gina Griggs, Commissioner

Selah Hmun, Honorary Commissioner

Kristin Einberger, Commissioner*

Karina Servente, Commissioner*

Janelle Sellick, Commissioner*

Open Space, Active Transportation, and Sustainability (OSATS) Commission (Formerly Open Space Advisory Committee)

Nance Matson, Chair

Barry Christian, Commissioner

Tara Clinton-Horner, Commissioner

Scott Artis, Commissioner

Kim Hester Williams, Committee Member*

Todd Novak, Committee Member*

**Active during original adoption (2018)*

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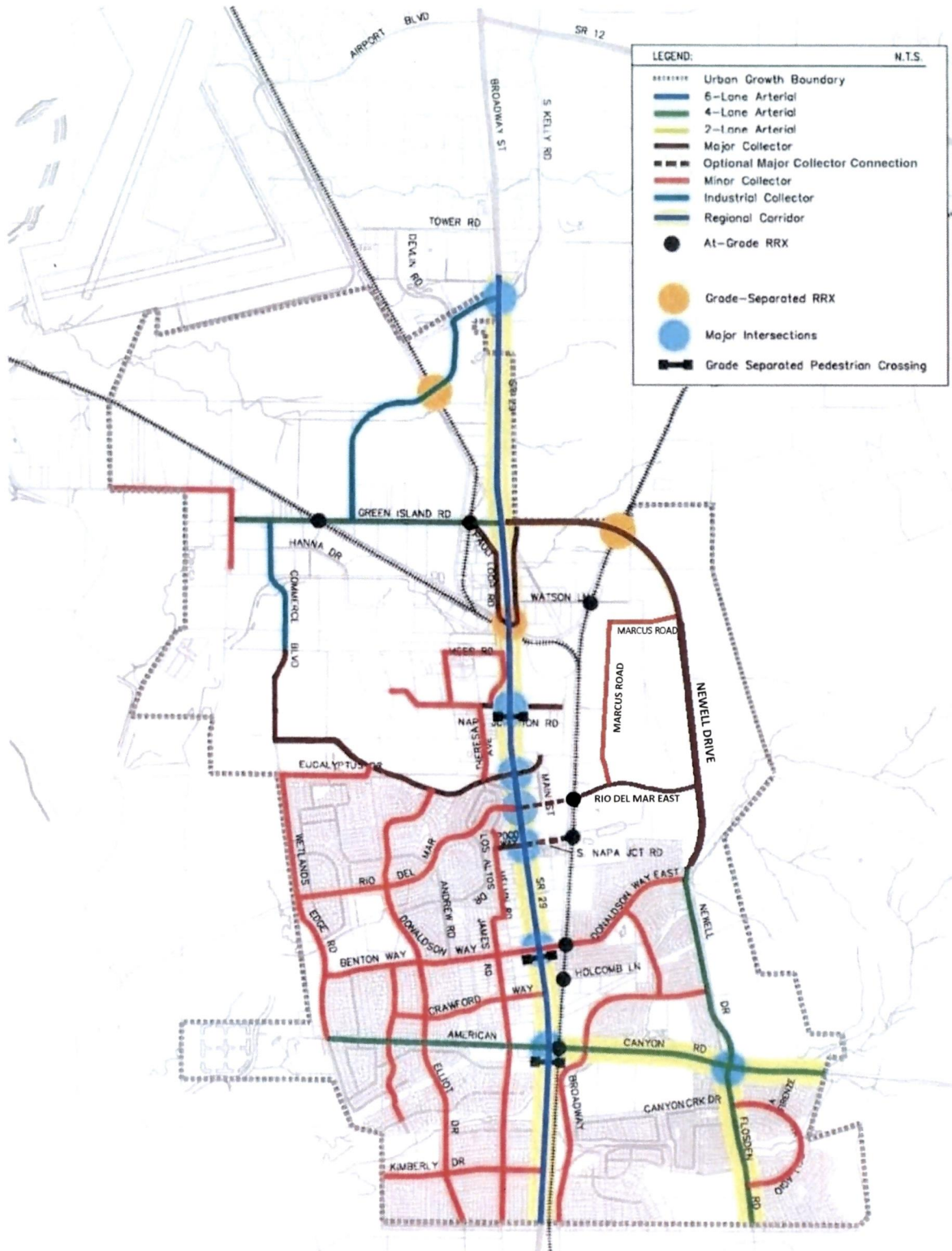
EXECUTIVE SUMMARY

The Watson Ranch Specific Plan (WRSP) Area is located within the city limits of American Canyon, in Napa County, California. The WRSP is a comprehensive planning document that will guide development of the approximately 309-acre WRSP Area. The WRSP Area is an important part of the City's General Plan and is intended to contribute significantly to the identity of American Canyon with its unique draw of new residential neighborhoods and the Napa Valley Ruins & Gardens (NVRG). The adaptive re-use of the NVRG will support community and private gatherings in a setting that is unlike anything else in the region. The WRSP completes the fabric of American Canyon by linking the community through roadways and regional trails and by creating a "Town Center" with community gathering places. New residential neighborhoods will provide a wide range of housing options. The WRSP development program includes the following elements:

1. A mix of commercial, recreational, and community uses in and around the NVRG that will consist of:
 - a. An adaptive reuse of the NVRG area to include a variety of commercial uses, including a combination of indoor and outdoor spaces for an amphitheater, restaurants, pubs, distillery, wine tasting, breweries, food trucks, farmers' market, weddings, and corporate/public/private events. Uses not listed in the NVRG may be allowed through the City development review process (e.g., Conditional Use Permit).
 - b. A boutique hotel with approximately 200 rooms and 30 private residences as part of the hotel.
 - c. The Quarry Lake, with park, trail and open space uses.
 - d. Mixed-use and/or live-work housing of up to 10% of total units (included in residential project total).
2. Residential development consisting of varying housing types and densities to accommodate the current and future demographic trends. The summary of residential uses proposed in the plan are:
 - a. Approximately 102 acres (623 units) of medium density residential (MDR-12).
 - b. Approximately 65 acres (630 units) of medium density residential (MDR-16).
 - c. Approximately 6 acres (186 units) of high density residential (HDR) affordable housing.
3. Community amenities consisting of:
 - a. Approximately 63 acres of parks and open space.
 - b. A community trail system that includes connecting portions of the Napa Valley Vine Trail and the River to Ridge Trail.
 - c. A community plaza and site for an approximately 20,000 square foot community center that will serve as a gathering place for American Canyon visitors and residents.
4. Infrastructure improvements that include:
 - a. The extension of Newell Drive along the WRSP Area's eastern boundary.
 - b. The extension of Rio Del Mar from SR 29 to the Newell Drive Extension, to be named "Rio Del Mar East."
 - c. A new rail crossing at the Union Pacific rail line at Rio Del Mar East or South Napa Junction Road.
 - d. Internal circulation for the WRSP Area.
 - e. The water distribution system, sanitary sewer system, storm water drainage system and dry utilities to service the WRSP Area.

Finally, the WRSP sets forth a conceptual phasing plan and a financing program to address the timing of build out and the funding of the infrastructure associated with the WRSP.

FIGURE 3:
CIRCULATION MAP



CHAPTER 1 - INTRODUCTION

1.1 Purpose

The Watson Ranch Specific Plan (WRSP) is a detailed planning document which provides the land use and policy framework for the future development of approximately 309 acres within the City of American Canyon. The WRSP Area is located toward the northeastern side of American Canyon and to the east of Lincoln Highway, which is also designated as California State Highway 29. It is generally bound by open space to the north and the Vintage Ranch residential neighborhood to the south. It is further framed by Union Pacific railroad tracks and existing development to the west, and the City limit line, grassland, and open space to the east. Within the City of American Canyon General Plan, the WRSP Area is designated as Town Center (TC). The City's objective for the WRSP Area as stated by its General Plan is to:

“Provide for the development of a Town Center that physically and functionally serves as the symbolic and identifiable focus of community activities and events for the City of American Canyon, and which is a regional destination within the Napa Valley (GP Objective 1.19)”

An extensive public outreach process, demographic and market research, and the physical characteristics of the land have molded the WRSP to foster a premier mixed-use community with a strong sense of place. The development in accordance with the WRSP will provide an array of new residential homes and services for both residents and visitors. The WRSP is consistent with the community vision of American Canyon, as a community with a small-town character and feel, socially and ethnically diverse, and ideally located in Napa County in close proximity to the Bay Area. The proposed mix of housing, retail, commercial, entertainment, and parks will further strengthen the existing community ties while also providing a community gathering place. The purpose of the WRSP is to provide a comprehensive set of goals, objectives, policies, development standards, and design guidelines for the development of the WRSP Area. The implemented WRSP will result in a unique and appealing new community which provides a variety of new amenities and infrastructure improvements necessary to implement the development. The organization of this document reflects the progression of the planning process that helped shape the WRSP, moving from an explanation of the over-arching goals to specific chapters that deal with certain topics such as land use, and circulation which have policies that implement those goals.

The WRSP details the following elements:

- A range of new housing types and densities including High Density Residential (HDR) and Medium Density Residential (MDR-12 and MDR-16) neighborhoods.
- The Napa Valley Ruins & Gardens (NVRG) area consisting of civic, retail, entertainment, commercial, visitor serving, and residential uses focused in and around NVRG.
- Community amenities, including parks, trails and open space areas.
- Necessary public and private infrastructure improvements, including streets, pedestrian circulation, water distribution system, sanitary sewer system, storm water drainage system, and dry utilities for the WRSP Area.



The WRSP provides the community vision, land use plan, circulation plan, development regulations, design guidelines, and implementation measures to ensure development in a manner that is consistent with the goals, objectives, principles, and policies of the City of American Canyon General Plan.

1.3.2 AMERICAN CANYON MUNICIPAL CODE

The City of American Canyon Municipal Code, Title 19 (Zoning Ordinance) provides standards for site-specific development and land use regulations that govern the size, shape, and type of use for development in the City of American Canyon. In any instance where the WRSP regulations and development standards may vary from the Zoning Ordinance, the WRSP will take precedence. Where the WRSP is silent on a topic, the City of American Canyon Zoning Ordinance requirements will remain in force.

The WRSP will be adopted by ordinance and become the controlling zoning for the WRSP property pursuant to City of American Canyon Municipal Code Title 19, Chapter 19.12. Town Center Zoning District.

1.3.3 NAPA COUNTY AIRPORT LAND USE COMPATIBILITY PLAN

The Napa County Airport Land Use Commission (ALUC) has the responsibility of preparing an Airport Land Use Compatibility Plan ensuring that land uses in the area surrounding the airport are compatible with airport operations. A very small portion of the WRSP Area is partially subject to airport restrictions for Airport Zone D in the northwestern corner of the WRSP Area. Under the Napa Airport Land Use Compatibility Plan, this zone restricts residential development. The proposed plan is compatible with this requirement. A majority of the WRSP Area is within Zone E where there are no land use restrictions.

1.3.4 RELATIONSHIP TO OTHER PLANS

The WRSP Area is located adjacent to the SR 29 Priority Development Area (PDA) designated by Association of Bay Area Governments and Metropolitan Transportation Commission. The goal of the PDA is to relieve congestion and adopt a complete streets concept for the section of Highway 29 through American Canyon, while promoting a healthy mix of uses surrounding the highway that support the various modes of transportation.

The Broadway District Specific Plan (BDSP) was adopted by City Council in 2019 and updated September 1, 2020. The WRSP and the BDSP represent parallel planning efforts that work together to meet regional housing requirements and provide the opportunity for improved transit and walkability

consistent with the City's General Plan. The Broadway District is a Priority Development Area (PDA) as designated by the Association of Bay Area Governments (ABAG) and Bay Area communities. PDAs are areas specifically designated for new housing and job growth with some incentives to encourage investment. PDAs are considered foundational for sustainable regional growth and Plan Bay Area. PDAs emphasize housing at higher densities near transit or transit corridors.

Both the WRSP and the BDSP are coordinated with one another, especially on the Rio Del Mar extension which connects Highway 29 to Newell Drive. The overall goal of both projects is to create a complete street concept with mixed use walkable neighborhoods that is consistent with the vision of the American Canyon General Plan within both project boundaries. The WRSP has planned connections to and segments of the Napa Valley Vine and River to Ridge Trails. The WRSP Area is a key part of these future trail extensions, and will provide an important part of the overall regional trail system. The Napa Valley Vine Trail is expected to link 47 miles of dedicated multi-use trails from Calistoga south to Vallejo Ferry Terminal crossing through the WRSP Area from the north and intersecting the River to Ridge Trail (running east/west) as it makes its way south. The Circulation Element was amended in October 2018, as specified by the 2018 WRSP, to incorporate an amended Circulation Map and amended Table 3 (Major Circulation Improvements).

1.4 Public Outreach

1.4.1 STEERING COMMITTEE

For development of the 2018 WRSP, the City appointed an eight person Steering Committee made up of two Council members, one Planning Commissioner, one member each from the Parks and Community Services Commission (PCSC) and the Open Space Advisory Committee (OSAC), a representative of the Napa Valley Unified School District and two residents at large. City Department Heads also attended the Steering Committee meetings. Over several meetings early in the WRSP preparation process, the Steering Committee identified the Planning Principles which defined the vision for the WRSP Area. These Principles have been restated in Section 1.5 below,

1.4.2 COMMUNITY INPUT

The City values public involvement in the planning process. So, in addition to the involvement of the Steering Committee throughout the WRSP process, an extensive Community Outreach Program was implemented including community workshops and opportunities to participate and comment online.

CHAPTER 2 - EXISTING CONDITIONS

2.1 Project Location

The City of American Canyon is situated along the southern tip of Napa County. Located on Highway 29, American Canyon is approximately eight miles south of the City of Napa and thirty-five miles northeast of San Francisco. The Watson Ranch Specific Plan (WRSP) Area occupies a

portion of the eastern edge of American Canyon within the City's urban limit line.

Figures 2.1 and 2.2 show the location of the WRSP Area in context with the surroundings, both regional and citywide.

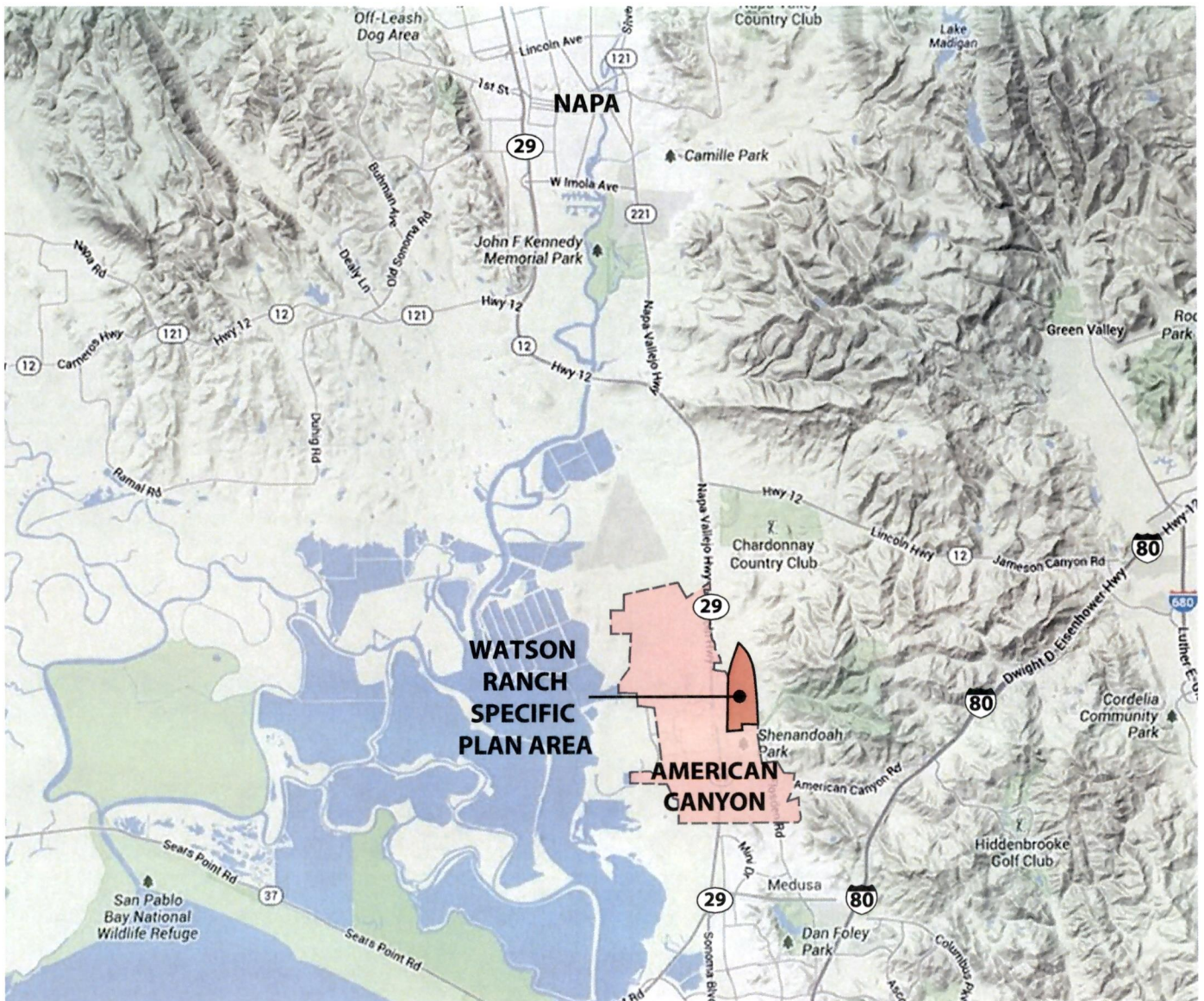


Figure 2.1: Regional Site Context

2.2 Ownership

The WRSP Area is approximately 309 acres of which approximately 252 acres is owned by American Canyon I, LLC (and its successor interests), while the remaining 57 acres is owned by the Newell Family interests. Figure 2.3

indicates the location of each individual ownership within the WRSP Area. American Canyon I, LLC, and the Newell Family interests are co-applicants for the WRSP.

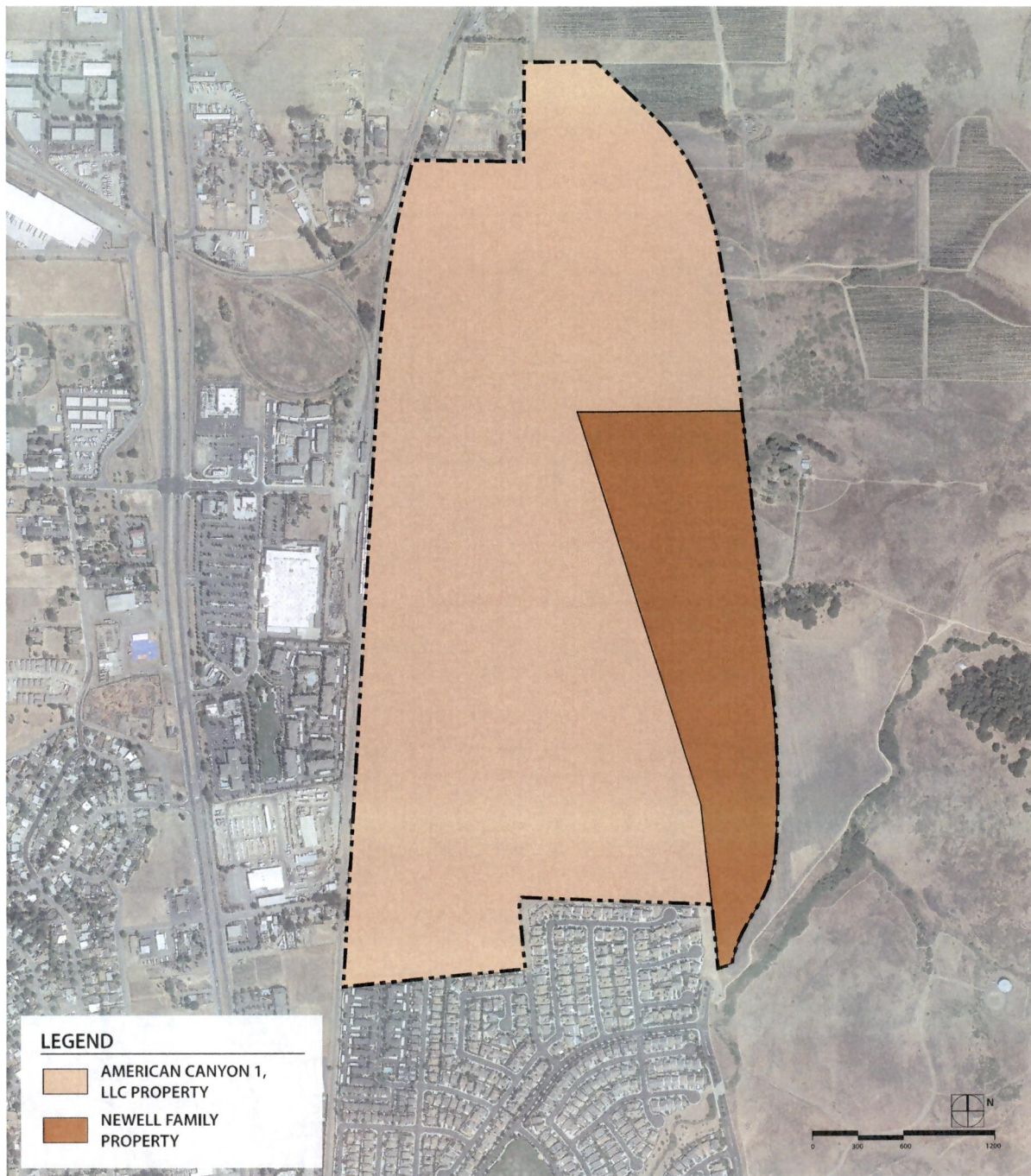


Figure 2.3: Property Ownership

and east. In general, elevations drop from the east to the west. The vegetation on-site is predominantly short seasonal grasses with some limited hay production. Trees on site are limited with a cluster of eucalyptus trees on the north end of the WRSP Area.

Located in proximity to the Napa Valley Ruins & Gardens (NVRG) is an approximately two (2) acre lake referred to as the “quarry lake” or “pond”. There are two minor wetlands that generally run east to west across the site between the PG&E Gas Line and the Union Pacific Rail line. Numerous field studies were conducted for the WRSP Area. Biological features including wetlands and special status species were evaluated and preliminary results are reflected on Figure 2.4. Additional analysis and information may be found in the WRSP Environmental Impact Report (EIR).

Existing structures on site include the substantial concrete structures in the south end known as the NVRG. These are the remnants of a Portland cement plant, a turn of the century industrial use which manufactured cement. The Napa County Airport is located approximately two (2) miles northwest of the WRSP Area. As a result, the WRSP Area is subject to Napa County Airport Land Use Plan Compatibility Zones. These zones are designed to address airport noise and safety concerns by restricting certain land uses that are incompatible with airport operations. A small portion of the northwestern corner of the WRSP Area is affected by Zone D, which substantially restricts residential development. No residential uses are proposed for this area of the WRSP. A large portion of the WRSP Area is within Zone E (see Figure 2.4). Zone E has no land use restrictions but requires review by the Airport Land Use Commission for any General Plan Amendment or Specific Plan Amendment or approval. Therefore, the WRSP was referred to the Airport Land Use Commission (ALUC) in 2016, and the ALUC made a determination of consistency for the WRSP with the Airport Land Use Plan. The amended WRSP was referred to the ALUC in 2022, and the ALUC made a determination of consistency for the amended WRSP with the Airport Land Use Plan.

2.4 Site History

The Standard Portland Cement Company opened in 1898 to manufacture cement by excavating clay & limestone, which created seven limestone quarries in the southern portion of the site within the WRSP Area. This company, with up to 400 employees working in 12-hour shifts, produced 2,000 barrels of cement a day. This cement was vital in helping rebuild San Francisco after the 1906 earthquake and fire.

In the 1930s the cement company closed. In the 1950s, the Basalt Rock Company took over and produced lightweight aggregate used in concrete for high-rise buildings. This continued until 1978 when the Basalt Rock Company closed.

The 1898-1930 industrial uses created most of the structures known as the “ruins” that remain on the site today within the NVRG. Because of this long time frame, the walls and facilities have an amazing texture and context from weathering leaving the aggregate rough and exposed, giving it the feel of something from ancient Rome or Greece.

In 1985, Jaeger Vineyards purchased the property. After failed attempts at grape cultivation, it was determined that the soils on the site will not support vineyards and the property should be put to other uses.

American Canyon incorporated as a city in 1992. Thirty acres of the subject site, in the vicinity of the ruins, were included within the newly formed City limits. The remaining acreage stayed in the unincorporated County of Napa. Interest in the property began to grow as the future site of a new Town Center and, in 1994, the City’s General Plan designated the property as the “Town Center”. In 1999, as a result of ongoing public support for the project, American Canyon voters approved “Measure C” with more than 83% of the vote. This citizen initiative pre-zoned 70 acres of the property to “Town Center”. That same year, Napa County LAFCO added the 70-acre portion to the City’s Sphere of Influence, making it eligible for annexation.

In 2008, an Urban Limit Line (“ULL”) was approved by the City of American Canyon. The ULL governs growth boundaries for the City through the year 2030. In 2010, LAFCO approved the annexation of the WRSP Area. A lot line adjustment establishing the approved City limits was recorded in 2011.

In 2012, a concept for adaptive reuse of the ruins within the NVRG as a local and regional destination was prepared. The City of American Canyon reviewed the concept as well as the feasibility study prepared by the consulting firm Economic and Planning Systems, Inc. (EPS). In 2013 the City of American Canyon updated its Circulation Element, which assumed the land uses set forth in the WRSP. In 2018 the Circulation Element was amended to incorporate amendments set forth in the 2018 WRSP. The Circulation Element (along with the Land Use Element) will again be amended to reflect land use and circulation changes set forth therein as described in subsequent chapters. Both the Circulation Plan in Chapter 7 and the Land Use Concept in Chapter 4 respond to the policies and standards included in the Circulation Element.

CHAPTER 3 - VISION AND DESIGN STRATEGY

3.1 Vision

The Watson Ranch Specific Plan (WRSP) promotes the development of a vibrant, progressive, and fully integrated community; one that will be recognized as a memorable place to live, work, and play for present and future generations. The overall plan will include a series of neighborhoods that integrate housing, recreation, retail, and commercial opportunities. The WRSP Area will be a vibrant, memorable place where civic, destination shopping, dining, hospitality, employment, residential, and recreational uses converge. This opportunity provides a community gathering space within the City of American Canyon serving visitors and residents while preserving American Canyon's small town feel and character. The execution of this character is governed by the Development Regulations in Chapter 6 and influenced by the Design Guidelines in Appendix A of the WRSP.

Key components to the success of this urban fabric include community threads such as pedestrian and bicycle friendly streets, open space connections, and vistas to important community monuments, such as the Napa Valley Ruins & Gardens (NVRG). These urban design features link neighborhoods into a cohesive community yet allow them to have individual character. These linkages and their focal points establish the framework and character of the new community. In addition to this community structure, strong and simple distinctive architecture and landscape themes will help to establish a unique identity.



Standard Portland Cement Company ruins, present day



Community event opportunities

not developed in tracts separated by walls. The modified grid also allows the neighborhood pattern to work with the existing topography.

The majority of the residential neighborhoods lie to the north of Rio Del Mar East and are accessed from Rio Del Mar East and Newell Drive via a minor collector road (Marcus Road). This road includes on-street striped bike lanes and six-foot wide sidewalks that encourage walking and cycling within the community to trails, parks, and the NVRG.

To further strengthen community connectivity, two regional trails, the River to Ridge Trail and the Napa Valley Vine Trail are linked to a series of internal trails that loop through the WRSP Area. Parks are strategically located along the primary trail linkages with one major park creating a larger combined central open space feature. A detailed description of the parks and trails is provided in Chapter 5.

The Napa Valley Vine Trail is a major north/south pedestrian and bicycle amenity forming the backbone of the community. In order to facilitate an optimum trail experience, road crossings are minimized to only two road crossings. Pedestrian and bicycle connectivity to the adjacent neighborhoods is seamlessly integrated, while east/west vehicular connectivity is minimized. To promote pedestrian and vehicular access to the Napa Valley Vine Trail, a variety of design techniques are used, such as single loaded streets, integration of the park and trail systems, and an extensive network of secondary trails and sidewalks that connect the neighborhoods with the regional trail. Section A.3.1 in Appendix A addresses how roads and homes interact with this trail corridor.

A palette of coordinated streetscape furnishings will enhance the WRSP's urban areas. Furnishings such as bike racks, drinking fountains, newspaper racks, trash receptacles, and benches are envisioned to be provided at appropriate locations.

The NVRG will function as a mixed-use Town Center and will be the focal point of the WRSP community where uses such as festivals, wine tasting, brew pubs, distillery, dining opportunities, hospitality, and retail all converge within the central organizing element of the NVRG structures. It is anticipated that this area will develop organically over time so the standards and guidelines in this document, while being directive, are intended to allow flexibility to encourage creative uses and design solutions in the adaptive reuse of the existing NVRG structures.



Coordinated streetscape furnishings



Appropriately located street furnishings; trail connections

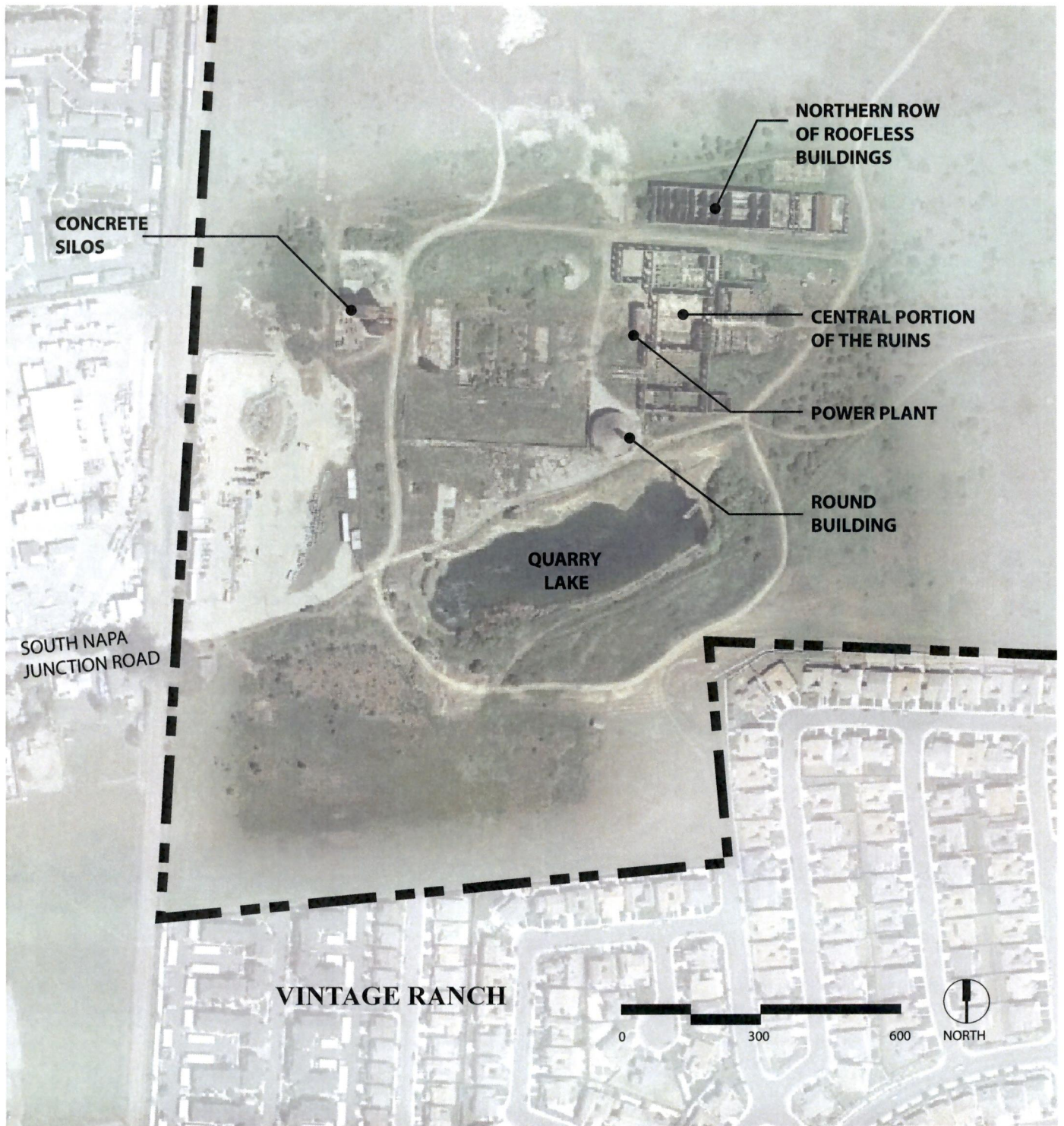


Figure 3.1: Existing Structures

The following are some examples of potential types of uses and amenities envisioned for the NVRG.

- **Chapel / Wedding Area.** Amenities such as a small structure and gardens may be used for weddings and other similar private gatherings.
- **Main Ruins Structure.** The main structure is envisioned to contain spaces, gardens and plazas for various events such as receptions, business venues and other private gatherings. While it is not envisioned that the main ruins structures would have roofs added to them, they may incorporate permanent or temporary tensile structures or other free-standing means to provide shelter from the sun or rain.

- **Restaurants.** The NVRG is an ideal location for one or more restaurants, providing a synergy with adjacent/ nearby social uses.
- **Café.** The NVRG is well suited for a café or similar use that encourages people to spend time socializing and enjoying the NVRG.



Ruins structures today



Potential use for weddings and events



Ruins walls form enclosed spaces or "cloisters" in the main ruins area

- **Mobile Food Truck Venue.** Mobile food trucks provide high quality, freshly prepared food and beverages and are untethered to a static location. In most cases they simply arrive at a destination such as a festival or park wherever they can because they are self-contained and are open for business. In response to this dining industry movement, an area for food trucks could be provided. This area may be supplied with power for the trucks, eliminating their need to use generators. While the location and amenity may end up being fixed to a particular area, the individual trucks will vary, thus allowing for variety and change in the offering and a dynamic food experience. Additionally, this area may serve as overflow or flex space when trucks are not present.



Potential area for outdoor performances

- **Children’s Play Area.** Play structures will contain play experiences that encourage discovery of the history of American Canyon, the WRSP Area and the Basalt Plant/Portland Cement Factory ruins.

- **Parking.** The manner in which parking is provided for the various uses in the NVRG is critical to creating a project that is both economically successful and a truly pedestrian oriented place. Parking will be provided on Rio Del Mar East as parallel or angled parking and in parking lots that support the various uses. Parking areas will be distributed throughout the NVRG to allow for proximity and ease of access and to reduce their scale. Parking for larger events may be provided in vineyard or orchard lots that when not in use, simply support the landscape character of the area.



Mobile culinary trucks

- **Hospitality / Hotel / Hotel Parking.** This use is proposed to be located on the north facing slope of the hill in the eastern side of the NVRG. This location provides commanding views and allows for the opportunity to provide adequate parking, and a bold landscape statement. The site terrain will be sensitively graded to provide a road to the hilltop site. The Hotel will be a “boutique hotel” of approximately 200 rooms, additional casitas, and supporting amenities. There will also be private residences located near the hotel on the south side. The use is intended to be complementary to and work in conjunction with the wedding and event center use in the NVRG.

- **Overflow/Event Parking.** Large community events and private events will take place at the NVRG. As these will be scheduled events and not daily activities, the parking needed to accommodate the influx of people can be provided in the orchard. When planted, the orchard could be laid out to allow appropriate spacing and surfacing (crushed rock or other appropriate multiuse surface). This would create an area that is functionally



Example gathering space

- **Charcuterie.** At the southwest corner of the group of ruins structures is a small concrete structure. This structure and location may be appropriate for artisan food uses such as a charcuterie.
- **Festival Space.** The space between the Quarry Lake Park and Community Gardens could provide a gathering space, where local festivals and community events could occur.
- **Quarry Lake Park.** The former quarry lake and its surroundings can provide a unique park and recreational experience. This park facility is proposed to be privately maintained and operated, yet accessible as a public amenity. Access to the water for recreational purposes will be restricted. On the west, south, and east sides of the lake will be a trail and viewing places. Picnic opportunities will be provided, predominantly at the east end where a natural bowl occurs. Specific programming of recreational facilities in public parks will be coordinated with the City Parks and Recreation Department. The trail will connect to the NVRG and the residential neighborhoods to the south and north.
- **Artisan Building.** The “Maker” movement is gaining momentum across the Bay Area and the country. Appreciation for hand crafted items of all types is on the rise. Providing a place for artisans (“makers”) to work their craft or art is consistent with the concept of the adaptive reuse of the ruins. It may also house a market similar to the Oxbow Market in Napa that establishes a place for local crafts people and artists to sell their wares, or to enjoy local food from those who produce it.
- **Live/Work Building.** There could be an opportunity for a future live/work building located in the NVRG.
- **Mixed Use Building.** There could be an opportunity for a future mixed-use building to house residential above restaurants, retail, or a variety of other uses that complement and add vibrancy to the NVRG.
- **Potential Bus Stop / Shelter.** Vine Transit has two bus routes through the City of American Canyon. For more on transit and public services refer to Chapters 7 and 8 of the WRSP. Should a bus route be established along Rio Del Mar East, a stop at the NVRG should be considered along the Rio Del Mar East frontage. Specific siting of a stop and shelter will be coordinated with the transportation agency at tentative map stage (see Implementation Chapter 9).



Existing graffiti art at NVRG



Existing Quarry Lake



Existing graffiti art at NVRG

removable bollards allow vehicular access to different areas as needed. For example, vehicular access for drop-off could be provided at the wedding chapel. When not being used for vehicular access, the space simply feels like part of the gardens, not an asphalt street or parking area.

3.3.3 COMMUNITY ART & SIGNAGE

Given the unique identity established by the NVRG, community art can have a complementary role in energizing and defining the plaza and surrounding spaces. Community art may take the form of tile mosaics on the buildings, interactive fountains, sculpture, custom street furnishings such as benches, trash receptacles, and manhole covers, or seasonal items such as a playful and unique banner program. It may also include preservation of particularly creative graffiti art on the ruins themselves, or wall space dedicated to shows of street art.

Signage guidelines included in Appendix A, Design Guidelines, Section A.5.5 of the WRSP are intended to guide merchants towards unique and individual signage as this is critical in supporting this unique character.

3.3.4 LANDSCAPE CHARACTER

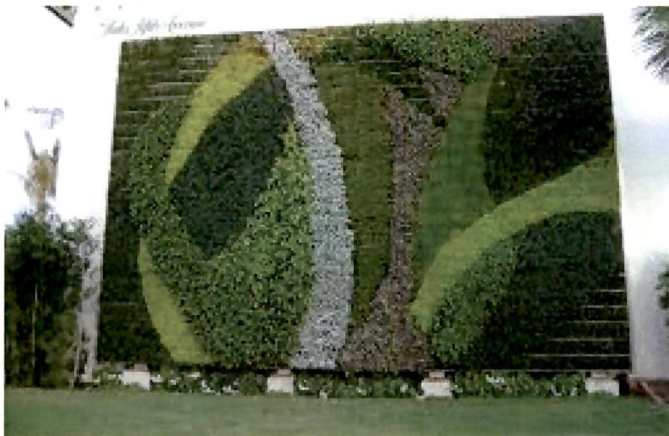
Supporting the unique architectural environment of the NVRG will be a strong landscape design concept. Plantings will be composed of drought-tolerant species. Landscaping of this area will draw heavily from classic images and include lavender, roses, grasses and flowering vines. Trees such as olives, in groves or strong linear patterns may be used to further reinforce the wine country character. Rows of vineyards create a strong geometric statement and support the wine country aesthetic. Consistent with the direction of the City's Steering Committee, the landscape and public spaces should celebrate the history of the place through thoughtful design and interpretive signage.

The planting palette and design will be robust to match the strength of the existing ruins structures. Blending structure and gardens, the massive walls of the ruins could support vertical gardens that create soft yet geometric planes of plantings. The opportunity for vertical landscaping (or "green walls") builds on the unique identity of the place. See Appendix A, Design Guidelines, Section A.5.3 for specific guidelines.

3.4 Neighborhood Design Concept

Residential neighborhoods within the WRSP Area will provide variety in terms of individual character. NVRG. Moving north away from the NVRG, lots may increase in size resulting in a transition to lower density moving away from the NVRG.

Neighborhoods are not walled off from each other and pedestrian connectivity between neighborhoods, amenities, and regional trails is provided. A hierarchy of streets is defined in Chapter 7 (Circulation), which shows where they occur in the community and if they provide through connections or simply serve a neighborhood of homes. The street system is based on the concept of "complete streets". Streets are strategically located to provide the greatest access to parks and regional trails. Open-ended cul-de-sacs will be utilized extensively to minimize the conflict for vehicles and pedestrians along major trail routes and to provide both neighborhood traffic calming and visible pedestrian access to amenities.



Vertical landscaping

of Craftsman and American styles. These styles will be reinforced with a carefully selected landscape palette that creates a memorable community with a strong sense of place compatible with the small-town character of the City of American Canyon. Refer to Appendix A, Design Guidelines.

3.6 Parks, Recreation, Open Space, and Trails Vision

The parks and open spaces for the WRSP Area form an integrated system with a wide variety of options for people to enjoy. In an urban neighborhood, this system is intended to provide more variety than a typical suburban park that is typically dominated by organized play fields. Park components within the WRSP Area include an active park in the central WRSP area abutting the Napa Valley Vine Trail, small private pocket/mini parks, and a park on the Newell Property – called Newell Park – bordered by Newell Drive to the east and Rio Del Mar East to the north. In the southern portion of the WRSP Area is a park surrounding Quarry Lake with passive recreational opportunities. Additionally, there are natural drainage areas and basins that are part of the overall WRSP Area drainage system that will include trails that are integrated into the trail network.

An important objective of the WRSP, consistent with the General Plan goal of creating a Town Center, is to provide a place that is available for events sponsored or scheduled by the City. The location for this community gathering space is to the east of the NVRG ruins at the intersection of Rio Del Mar East and Newell Drive. A future community center building will look out onto a community plaza. The

community center and plaza are integrated with Newell Park (see Chapter 5 for further discussion).

Many of these recreational amenities can be linked together by a series of looping pedestrian trails and bikeways, providing access to the immediate neighborhoods as well as connections to the regional amenities of the Napa Valley Vine Trail and the River to Ridge Trail.

The vision for recreational amenities with the WRSP Area also includes the development of small, privately owned and maintained pocket or mini parks within the neighborhoods. These parks may be as small as a single lot and be passive in nature but provide important places of urban relief within their context.

Community gardens in the NVRG will build and support a sense of community around local food sources and stimulate events in support of health and sustainability. The community gardens may also be available for partnership with food and



Community gathering area



Large active play areas



Pedestrian and bike paths

CHAPTER 4 - LAND USE

4.1 Introduction

The proposed Land Use Plan for the Watson Ranch Specific Plan (WRSP) implements the City of American Canyon's General Plan land use designation of "Town Center" (see Figure 4.1), which calls for a variety of residential densities and mix of land uses. The organization of land uses responds both to the physical features of the site and the policy framework of the General Plan.

The formation of the Land Use Plan (Figure 4.2) was a response to four key factors: (1) the policies set forth in the American Canyon General Plan, (2) the incorporation of the existing Napa Valley Ruins & Gardens (NVRG) structures and quarry lake, (3) the provision of higher residential densities around the NVRG, transitioning to lower densities, and (4) the site constraints such as the PG&E gas easement and wetlands.

Any lawfully existing land use occurring at the adoption date of the WRSP may be continued, notwithstanding any omission of a particular use in the Permitted Uses Section.

4.2 Land Use Goals and Objectives

The following goals, objectives, and policies have been prepared to establish the implementation framework for land use oriented Planning Principles found in Chapter 1.

Goal 4A – Provide an integrated and diverse mix of land uses.

Objective 4.1 – Provide integrated neighborhoods that contain both single-family and multifamily residential.

Policy 4.1.1 – Residential neighborhoods shall provide a broad range of housing types, including detached single family, attached single family, townhouses, condominiums, and apartments.

Policy 4.1.2 – The highest residential density shall be located around the NVRG with a transition to lower density in neighborhoods further away.

Policy 4.1.3 - Within the MDR-16 and/or the HDR residential categories, a diversity in the mix of residential housing types shall be achieved by including both townhome and condominium housing unit types to be constructed as determined by the market. Evaluation of the product mix will occur on a phase by phase basis to ensure adequately integrated neighborhoods with a variety of housing types.

Objective 4.2 – Create a pattern of land use development that respects the environmental setting of the WRSP Area.

Policy 4.2.1 – The environmental resources of the City shall be protected including significant wildlife habitats and vegetation, hillsides and canyons, creeks, rivers, and wetlands.

Policy 4.2.2 – For the portion of the WRSP located within Airport Overflight Zone E, aviation easements shall be required. Prior to development or with the recording of Final Maps in any given phase, whichever occurs first, aviation easements shall be recorded on all existing and proposed parcels located within the given phase.

Objective 4.3 – Provide for recreational, institutional, commercial and service uses that support resident needs within or in close proximity to residential neighborhoods.

Policy 4.3.1– Neighborhood serving retail and commercial use shall be located in the NVRG.

Policy 4.3.2 – Parks, trails and open space amenities shall be interconnected as an open space system and integrated into the fabric of the residential neighborhoods.

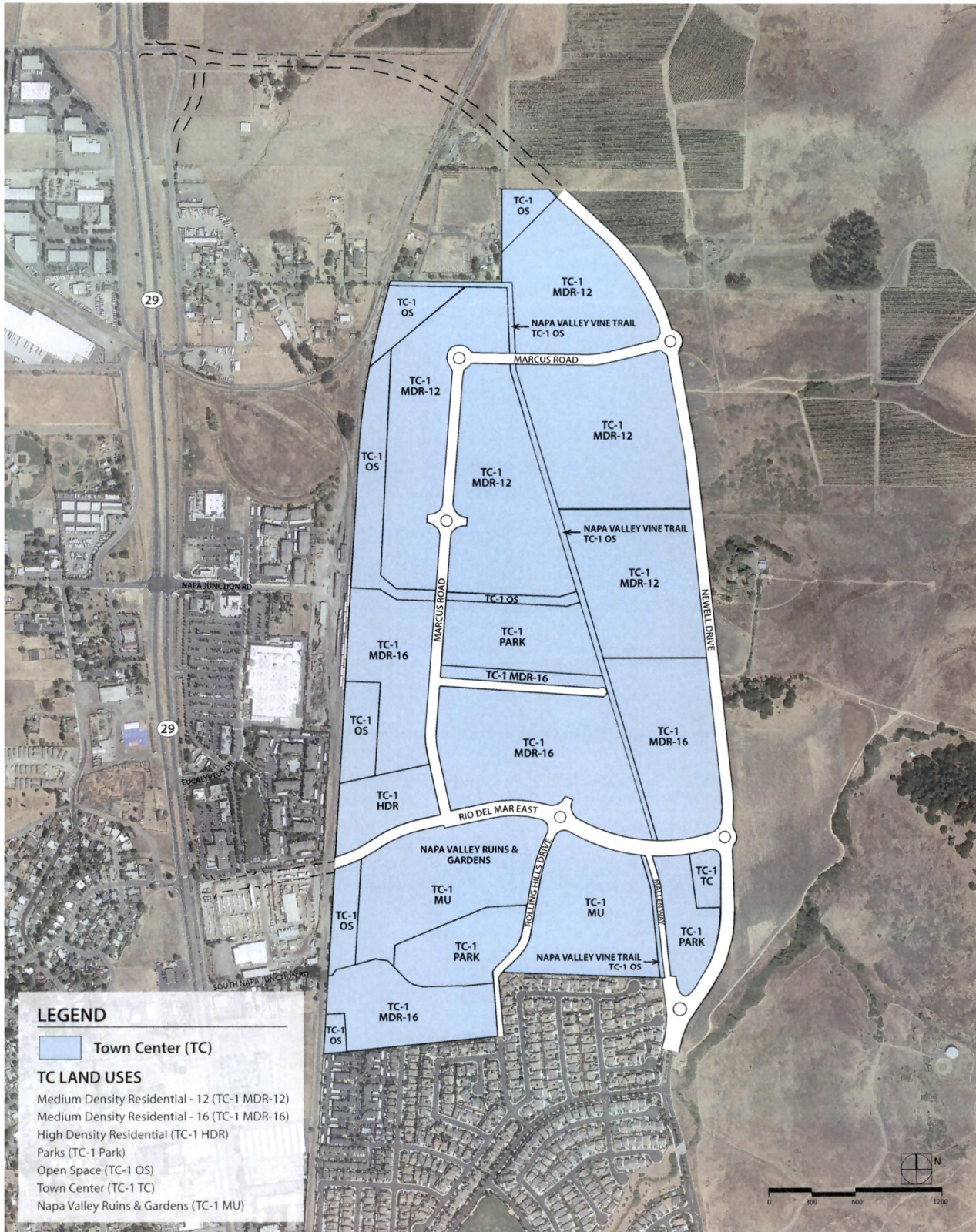


Figure 4.1: Town Center (TC-1) General Plan Land Use Map

American Canyon I, LLC Property		
Land Use	Area (+/- Acres)	Anticipate Development Intensity
Medium Density Residential (MDR-12)	86.5	503 units
Medium Density Residential (MDR-16)	53.3	507 units
High Density Residential	6.0	186 units (Affordable)
		200 room hotel
Mixed Use	38.6	176,000 SF Retail/Commercial 58,000 SF Ancillary Commercial
Parks	18.0	
Open Space and Trails	14.9	
Detention	9.4	
Wetlands	3.1	
Roads	22.0	
Total	251.8	1,196 units 200 room hotel 176,000 SF Retail/Commercial 58,000 SF Ancillary Commercial
Newell Family Property		
Land Use	Area (+/- Acres)	Anticipate Development Intensity
Medium Density Residential (MDR-12)	16.0	120 units
Medium Density Residential (MDR-16)	12.1	123 units
Parks	7.4	(Includes 2.0 acre Community Center)
Open Space and Trails	7.5	
Detention	3.3	
Roads	10.6	
Total	56.9	243 units

Table 4.1b: Land Use Summary Table by Ownership

4.4.1 SPECIFIC PLAN LAND USE DESIGNATIONS

The WRSP establishes a land use and regulatory framework within the parameters of the General Plan that allows a maximum of 1,253 residential units (plus additional units allowed by density bonus and related laws), 200,000 square feet of commercial/retail uses, and a 200-room hotel in the WRSP Area. Figure 4.2 identifies the location of the land uses within the WRSP Area.

Below, each Land Use Category and the intended uses are described in more detail. These land use designations are consistent with the overall General Plan Land Use designation of “Town Center” and serve to refine the land use configuration for the WRSP Area. As described in Section 4.3.2 above, a General Plan Amendment to further define the specific land uses set forth by this specific plan under the Town Center Land Use Designation and supporting policies, amends the General Plan Land Use Map to incorporate the land uses with the prefix TC-1 thereby denoting the WRSP land uses (see Figure 4.1). This amendment recognizes and ensures the consistency between the City of American Canyon’s General Plan and the WRSP, as required by controlling law.

These land use designations also serve as the “zoning” for the WRSP in reference to the allowable uses described further in Section 4.6, the Development Regulations set forth in Chapter 6, and the Design Guidelines in Appendix A. Where the WRSP and the City zoning regulations may conflict, the WRSP standards will apply. Where the WRSP is silent, the City Zoning regulations will apply.

RESIDENTIAL LAND USE DESIGNATIONS

The City’s General Plan requires that the Specific Plan(s) for the Town Center land use designation establish the densities and intensities of land uses. For the residential land uses, the WRSP sets forth three residential categories with varying density ranges - Medium Density Residential (MDR-12 and MDR-16) and High Density Residential. The density ranges for the two Medium Density categories overlap slightly with the intention of encouraging a higher diversity of housing product types within single neighborhoods, allowing greater flexibility to respond to market forces and creating an opportunity to achieve what is referred to as the “Missing Middle” of housing types. The “Missing Middle” is a range of multi-unit or clustered housing types compatible in scale with single-family homes that help to achieve workforce housing and promote vibrant walkable residential neighborhoods. At the same time, the densities transition from higher to lower away from the NVRG consistent with the General Plan Policies.

Medium Density Residential (MDR-12): This Medium Density Residential is intended for single-family homes

with a permitted density between 2 to 12 dwelling units per gross developable acre.

Medium Density Residential (MDR-16): This Medium Density Residential is intended for areas of medium density detached and attached housing such as small lot single family homes, duets, duplexes, three-plex, four-plex, townhome and condominium units. The allowable density range is 8 to 16 dwelling units per gross developable acre.

High Density Residential (HDR): High Density Residential is intended for areas of high-density attached housing such as, townhomes, apartments, and condominiums. Per the General Plan, this residential category density shall be provided at a minimum of 20 dwelling units per gross developable acre.

NON-RESIDENTIAL LAND USE DESIGNATIONS

Napa Valley Ruins & Gardens (MU): The MU designation includes the former site of the Standard Portland Cement Factory. It comprises approximately 39 acres. The MU designation is a mixed-use designation made up of several different uses including but not limited to commercial uses (such as retail, restaurants, personal services, dining, wineries, breweries, pubs, distillery, mobile food trucks, and farmers markets), mixed-use residential (including live/work), and office uses. It also includes visitor serving uses such as hotel, events center, entertainment, and recreation. Finally, it permits civic uses such as a community plaza, community center and parks. Development within this area may be comprised of single use buildings or buildings containing multiple uses including mixed use and/or live-work housing of up to 10% of total units (included in the residential project total).

Parks (Park) and Open Space (OS): The Park designation allows for a variety of recreational uses such as the lake, multipurpose trails, tot lots, playground equipment, passive and active recreational amenities such as ball fields, courts, skating ramps, and picnic facilities.

The Open Space (OS) designation includes privately or publicly owned property to be retained for open space purposes such as resource management, storm water management and environmental mitigation. Open space uses may also include passive recreation (such as hiking, walking and biking trails, and nature observation). Passive recreational uses associated with open space areas while encouraged are not eligible for parkland credit.

The Parks and Open Space land uses may also contain civic or community uses such as community gardens, vineyards, orchards, and other edible landscape elements.

Town Center (TC): The Town Center designation allows for community uses such as a community plaza, and community center.

Use Classification	MU (NVRG)	MDR-12	MDR-16	HDR	PARK & OS	TC
RESIDENTIAL						
Single-family residential with a minimum density in this district is between 2 and a maximum of 12 dwelling units per gross acre and a minimum lot size of 3,200 sf. For other allowable and conditional uses refer to RS-6500 zone district in the City of American Canyon Municipal Code.	-	P	P	P	-	-
Medium Density Residential is intended for areas of medium density detached and attached housing such as small lot single-family homes, duets, duplexes, three-plex, four-plex, townhome and condominium units. The allowable density range is 8 to 16 dwelling units per gross developable acre.	P	-	P	P	-	-
High-density attached housing such as townhomes, apartments and condominiums. The allowable density range is a minimum of 20 dwelling units per gross acre. For other allowable and conditional uses refer to RH zone district in the City of American Canyon Municipal Code.	P	-	-	P	-	-
Mixed Use and Live/work residential units	P	-	C	C	-	-
Secondary living units. Secondary living units do not count as a separate unit from the primary residence.	C	C	C	C	-	-
RECREATIONAL						
Active and passive public recreational uses but not limited to multipurpose trails, tot lots, playground equipment, passive and active recreational amenities including, but not limited to, ball fields, courts, skating ramps, picnic facilities, and recreational use structures. Civic or community uses including but not limited to community gardens, vineyards, orchards and other edible landscape.	P	C	C	C	P	C
Active and passive recreational uses such as play areas, community gardens, swimming pools, etc. that are provided as part of an HOA and are privately maintained.	P	P	P	P	-	-
Environmental mitigation lands, drainage and detention/retention basins, drainage system appurtenances such as culverts, wetlands, and natural drainage ways. Also permitted are pedestrian and bicycle trails, viewing areas, interpretive signage, and site furnishings such as benches, and trash receptacles.	P	P	P	P	P	P
Recreation uses such as community plazas and small pocket parks.	P	P	P	P	P	P
Community gardens.	P	C	C	C	C	C
PUBLIC						
Civic Uses	P	-	-	-	-	C
Educational and institutional uses.	P	-	-	-	-	P

Table 4.2 (continued): Permitted and Conditionally Permitted Uses

of the original gross acreage approved under the WRSP, whichever is less. A revised Land Use Plan as well as a revised Land Use Summary Table must be submitted to the City of American Canyon for each proposed amendment or set of amendments to the land use area boundaries (see Implementation Chapter, Section 9.6.3)

4.5.2 TRANSFER OF DWELLING UNITS

The transfer of dwelling units between land use areas is permitted provided that there is no net increase to the total dwelling units permitted in the WRSP. A revised Land Use Plan as well as a revised Land Use Summary Table must be submitted to City of American Canyon for each proposed transfer of dwelling units, and the phasing plan and schedule adjusted accordingly (See Implementation Chapter, Section 9.8).

4.6 Permitted and Conditionally Permitted Uses

Table 4.2 summarizes permitted and conditionally permitted uses within the WRSP Area. While the descriptions are fairly comprehensive, they are not intended to prohibit an omitted but related use. Any uses not specifically identified here shall be considered through subsequent approval processes as defined in the Implementation Chapter.

CHAPTER 5 - PARKS AND OPEN SPACE

5.1 Introduction

The proposed parks, trails, and open space plan for the Watson Ranch Specific Plan (WRSP) includes a variety of parks, trails, and open spaces for a wide number of active and passive recreational use. The integrated network of parks, trails, and open space, both local and regional, is one of the fundamental design principles for WRSP providing the residents and the larger community recreational throughout the plan area. Approximately 63 acres of recreation facilities, park land, trails, and open space areas are provided within the WRSP Area. The parks, trails, and open space system consists of a series of inter-connected parks and trail types ranging from pocket parks to community parks with linkages to larger regional facilities such as the Napa Valley Vine Trail, River to Ridge Trail, and the Newell Open Space Preserve. Parks are programmed with a variety of uses to support activities throughout all seasons of the year. A range of park and trail typologies are distributed throughout the WRSP Area in order to provide ease of access and close proximity to all residents. The character of each park is based on the use and the role it plays within the overall open space network. They are a combination of the natural and the manicured; the informal and the formal; and the active and the passive, depending upon their location and functions.

The trails system consists of a combination of paved and unpaved trails that link major destinations within the WRSP Area, such as parks and the Napa Valley Ruins & Gardens (NVRG). The internal project trail system includes segments of regional trails (the Napa Valley Vine Trail and the River to Ridge Trail) and connects to the Newell Open Space Preserve. The network forms a series of loops of differing lengths. The loops provide alternative routes and allow choices between short, medium, and long distances for recreation and exercise. The trails run through various parks and open space conditions and their design reflects these differing conditions. The trails provide for a range of user groups including bikers, runners, and pedestrians.

The parks and open space system is designed to provide easy and convenient access to all residents. Most homes within the WRSP Area are no more than ¼-mile walking distance from a park, natural open space area, or trail alignment. This public network serves to encourage walking and to provide opportunities for a convenient and healthy lifestyle choice for all residents.

5.2 Goals, Objectives, and Policies

Goal 5A: Integrate Parks, Trails, and Open Space into a diverse system of active and passive recreation amenities designed to fulfill the needs of residents and support healthful, active lifestyles.

Objective 5.1 – Provide public parks distributed throughout the WRSP Area that include a range of recreational opportunities adequate to support the future population of the WRSP Area.

Policy 5.1.1 – Parks shall meet at minimum the City wide standard of 5 acres per 1,000 residents in the WRSP Area.

Objective 5.2 – Parks, Trails and Open Space will be designed as an integrated system of recreational amenities.

Policy 5.2.1 – Parks shall be programmed and designed through cooperation with the City of American Canyon Parks and Recreation Department and the general public and shall reflect the results of the City's Parks and Recreational Needs Assessment.

Policy 5.2.2 – Parks shall be centrally located in the WRSP Area with access afforded to a majority of the area residents via trail and street access.

Policy 5.2.3 – The Napa Valley Vine Trail and the River to Ridge Trail shall be interconnected and aligned to integrate with the primary parks where feasible in the WRSP Area.

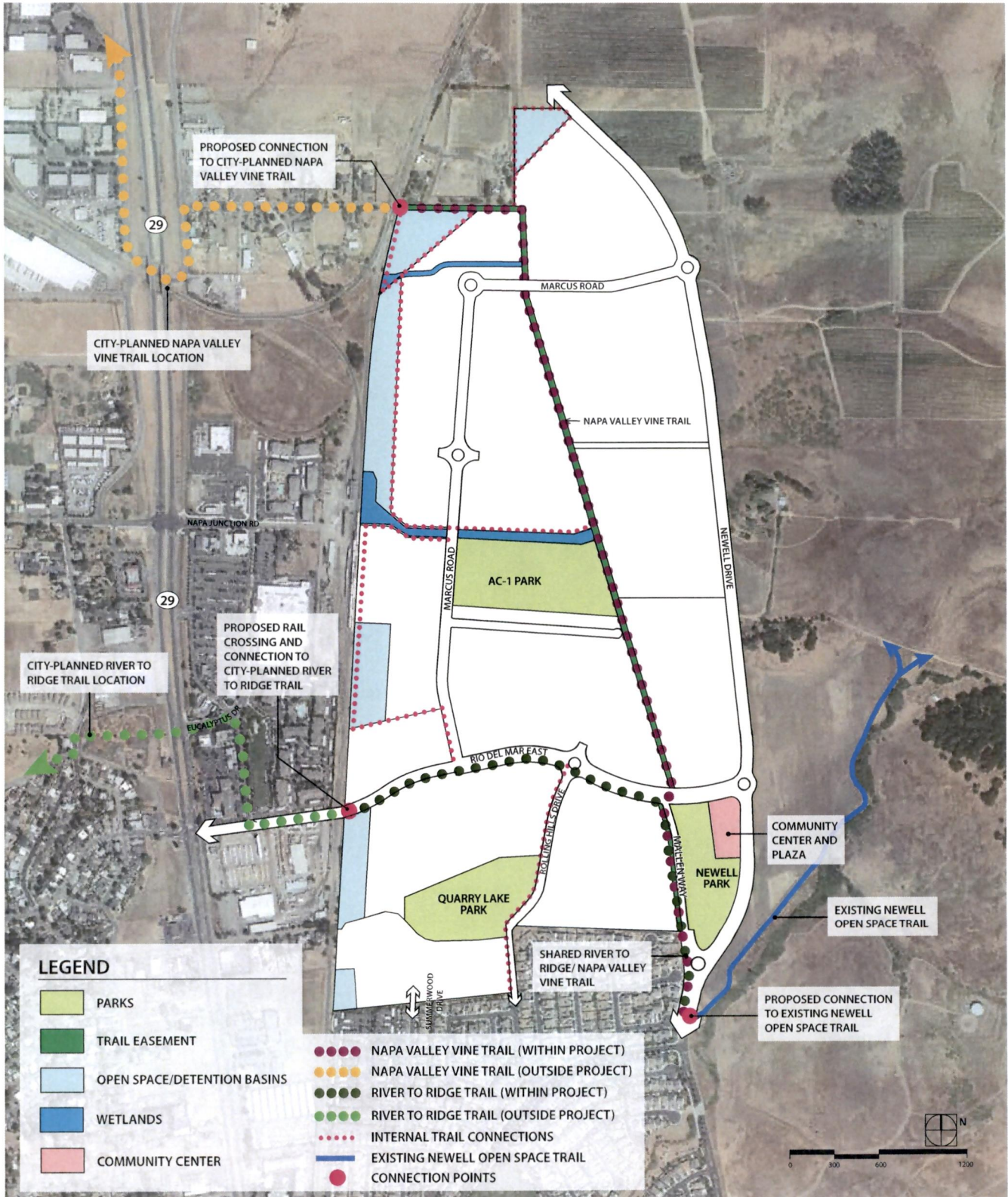


Figure 5.1: Parks Typology Map

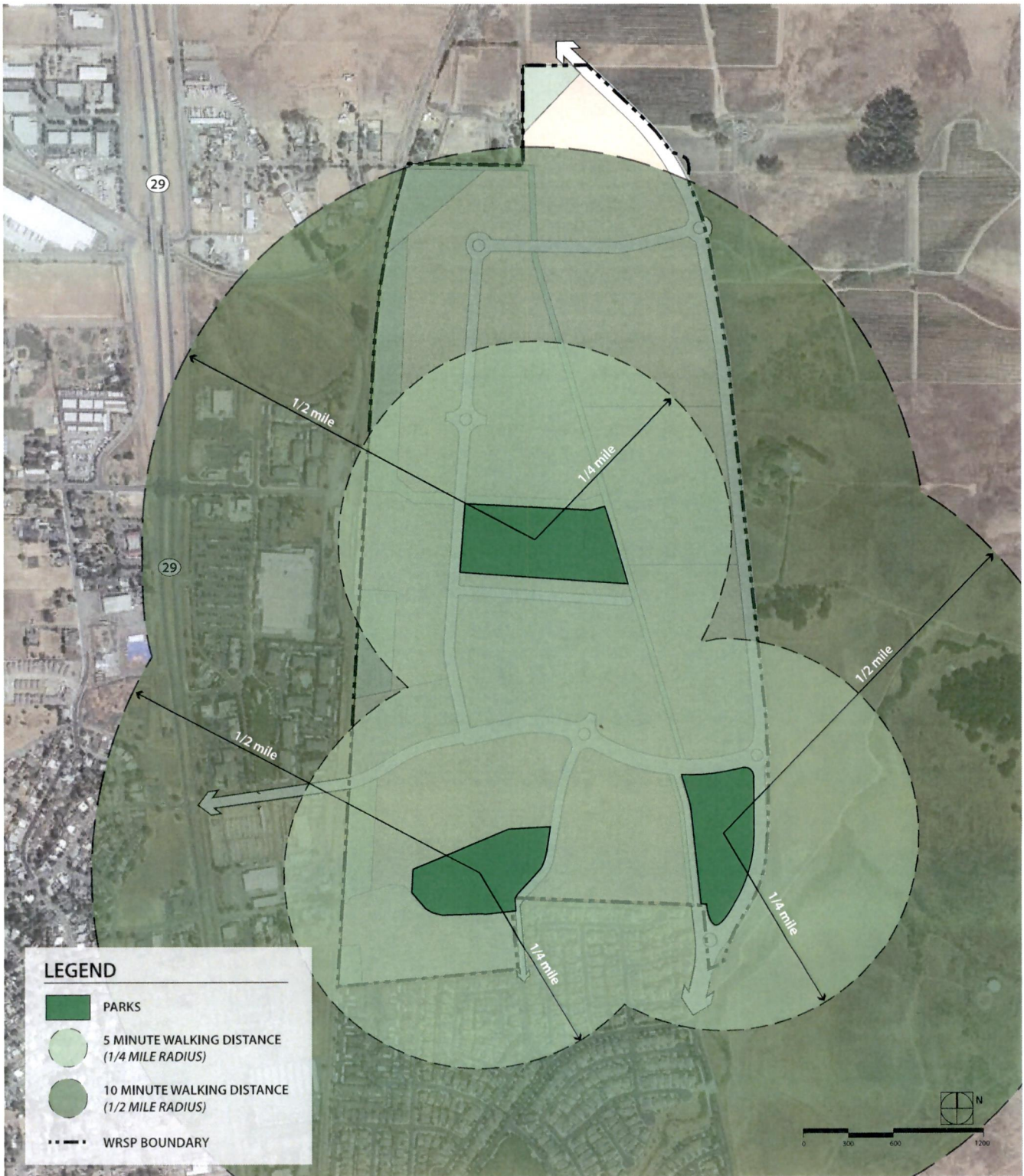


Figure 5.2: Parks Proximity Map

- Drought tolerant plant species
- Informal tree clusters
- Enhanced riparian habitat plantings around lake edge
- Landscaping that appears natural vs. manicured
- Accent (manicured) planting at entries
- Limited amount of turf grass
- Low maintenance native grasses
- Downward facing pole-mounted lights and bollards for safety
- Accent lighting on special landscape elements and park features – used sparingly
- Trail connections to the neighborhoods to the south

AC-1 PARK

This centrally located park is approximately 11.3 acres. AC-1 Park should be programmed with both passive and active uses. Level turf areas should be provided for active uses such as soccer and ball fields as well as smaller areas for passive recreation. The park should be buffered from the street with shrubs or earth berms. Flowering accent trees can be located to provide landmark areas and focal points, while a long windscreen run of tall narrow trees along the westerly boundary can screen the adjacent uses.

The following program elements and amenities should be considered for AC-1 Park:

- Playground
- Sports fields (soccer, baseball)
- Picnic facilities
- Community gardens
- Downward facing safety lighting
- Pathways
- Benches
- Trash receptacles
- Bike racks
- Drinking fountains
- Enhanced materials at entry nodes and seating areas
- Incorporation of the Napa Valley Vine Trail
- Wayfinding and educational signage
- Public restroom
- Downward facing pole-mounted lights and bollards for safety
- Accent lighting on special landscape elements and park features – used sparingly



Pedestrian path along water feature

5.4.3 POCKET PARKS AND MINI PARKS

These small passive spaces provide urban relief within a neighborhood. They will be privately owned and maintained and passive in nature. They may be as small as an individual lot and generally include: landscaping that complements the neighborhood, a play structure, turf, and benches or seat walls. While not required they are encouraged throughout the neighborhoods.

5.4.4 OPEN SPACE AREAS

Unprogrammed open space is located along the periphery of the WRSP Area and in transitional landscape areas. The areas comprised of level-to-moderate slopes can be planted with a simple understory mass of evergreen groundcovers and low to medium grasses: A single species of tree may be planted in rows symbolic of orchards and vineyards throughout northern California.

The Open Space Area landscape program may include, but not be limited to the following:

- Native and non-native non-invasive drought tolerant grasses.
- Native and non-native drought tolerant evergreen groundcover.
- Native and non-native trees planted in rows.
- Low maintenance landscape.

Open space areas may include:

Seasonal Wetlands - The seasonal wetlands and their surrounding buffer areas protect the existing sensitive

areas within the WRSP Area. They take advantage of natural systems to capture and clean storm water for the community while also providing for passive recreation. These areas will be connected to the rest of the open space system with multi-use trails.

Detention Basins - Detention basins are designed to be multiuse. During the dry months, when not being used for the purpose of detaining stormwater flow, they may allow for some recreational activities. The appropriate design, plant material and program elements make these a valuable component of the open space network providing both physical and visual benefits that are associated with open space.



Naturalized Planting



Open Play Area



Children's Play Area

5.6 Pedestrian and Bicycle Circulation

Pedestrian and bicycle circulation is integral to the overall open space and park network. Refer to Chapter 7 Circulation for details.



Park Furniture



Bike Racks

CHAPTER 6 - DEVELOPMENT REGULATIONS

6.1 General Provisions

6.1.1 PURPOSE AND INTENT

The Development Regulations set forth in the Watson Ranch Specific Plan (WRSP) provide a comprehensive set of regulations governing the use and development of the land. While including references to specific provisions of the American Canyon Zoning Ordinance, these Development Regulations replace the American Canyon Zoning Ordinance within the WRSP Area. Should there be a conflict between the WRSP and the American Canyon Zoning Ordinance, the provisions of the WRSP shall govern. Any issue not directly or specifically covered by the WRSP shall be subject to non-conflicting regulations and procedures of the American Canyon Zoning Ordinance.

6.1.2 SEVERABILITY CLAUSE

If any term or provision of the WRSP, or the application of any provision of the WRSP to a particular situation, shall for any reason be found to be void, invalid, illegal or unenforceable by a court of competent jurisdiction, all other terms and provisions of the WRSP or the application of the WRSP to other situations shall remain in full force or effect.

6.2.5 SETBACKS AND IRREGULAR LOTS

Setbacks are measured from the appropriate front, side, or rear lot line, or ultimate street right of way line, whichever is most restrictive. Irregular lots are defined as lots that are non-rectangular, lots with three sides, or lots with more than four sides that require specific definition of lot lines in order to achieve the purpose of the specific setbacks. Refer to Figure 6.1 for measuring setbacks on irregular lots.

Front Lot Line: On an interior lot, the line separating the lot from the street. On a corner lot, the shorter lot line abutting a street. On a through lot, the lot line abutting the street or lane providing the primary access to the lot. On a flag or panhandle lot, the interior lot line most parallel to and nearest the street or lane from which access is obtained.

The width of the “flag” must be a minimum of 20’ and can be no longer than 2 times the width of the lot it serves. One “flag” driveway may access a maximum of 2 lots.

Rear Lot Line: The lot line that is opposite and most distant from the front lot line. In the case of an irregularly shaped lot, a minimum ten-foot-long line which is within the lot and parallel to and most distant from the front lot line shall be considered the rear lot line for purposes of determining setback.

Side Lot Line: All lot lines, which are not front or rear lot lines, shall be considered side lot lines for setback purposes.

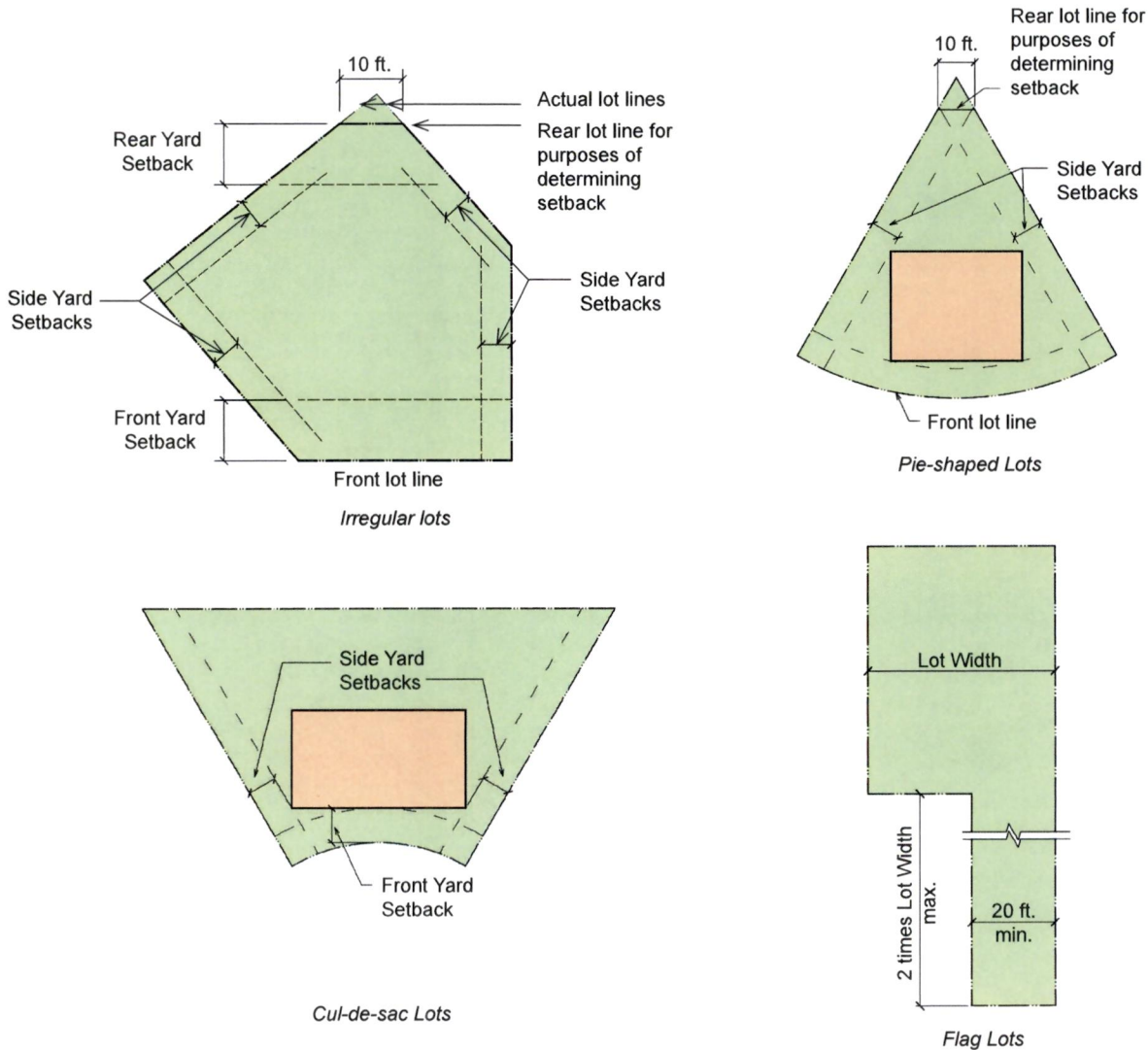



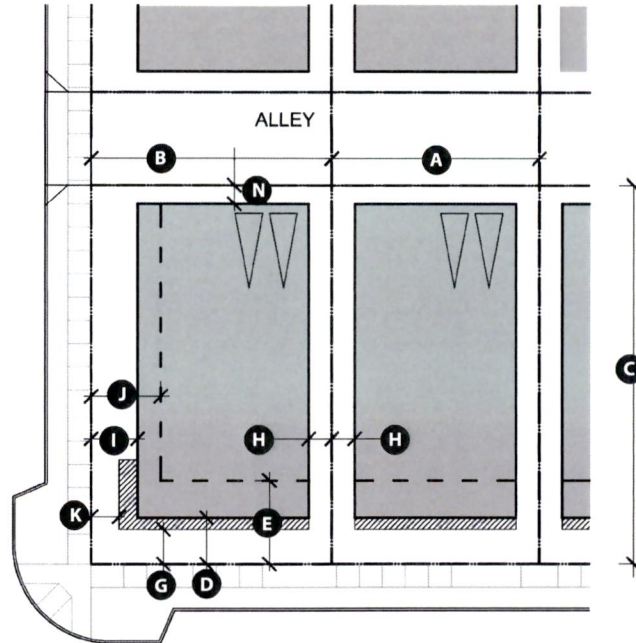
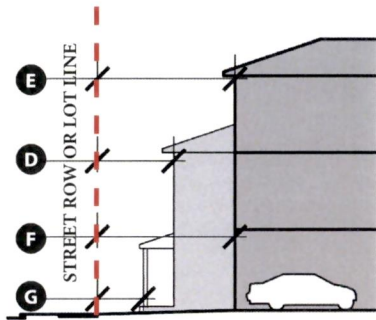


Figure 6.1: Irregular Lots: Measuring Setbacks

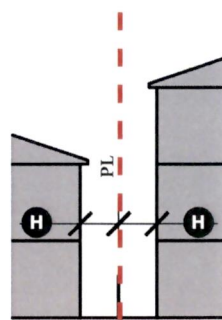
-  PORCH
-  BUILDABLE ENVELOPE LIMITS
-  PROPERTY LINE



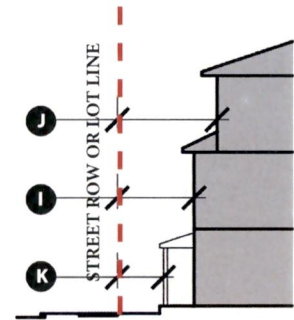
Medium Density Residential (MDR-12) - Alley Loaded



Front setbacks






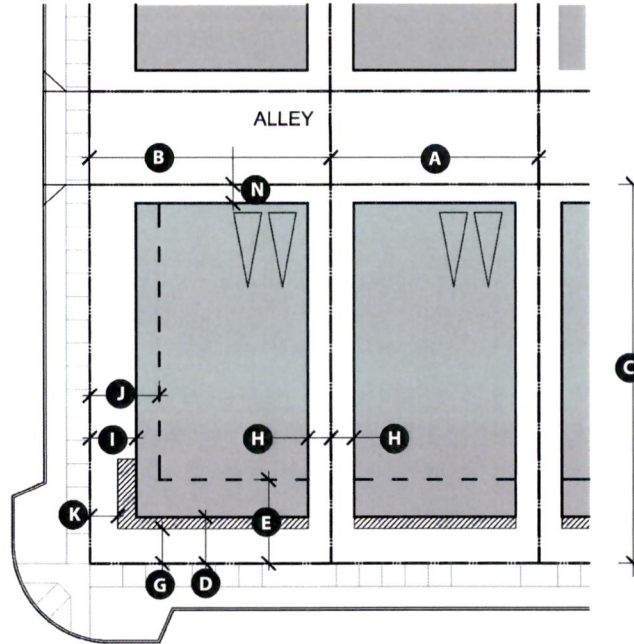
Side setbacks at internal lots



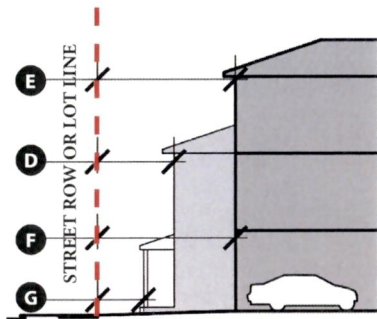
Side setbacks at corner



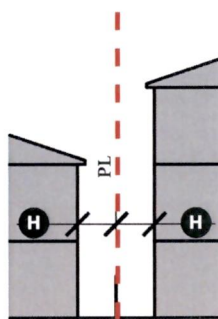
-  PORCH
-  BUILDABLE ENVELOPE LIMITS
-  PROPERTY LINE



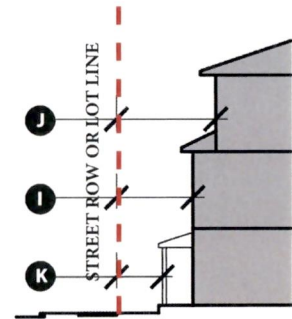
Medium Density Residential (MDR-16) - Detached, Alley Loaded



Front setbacks






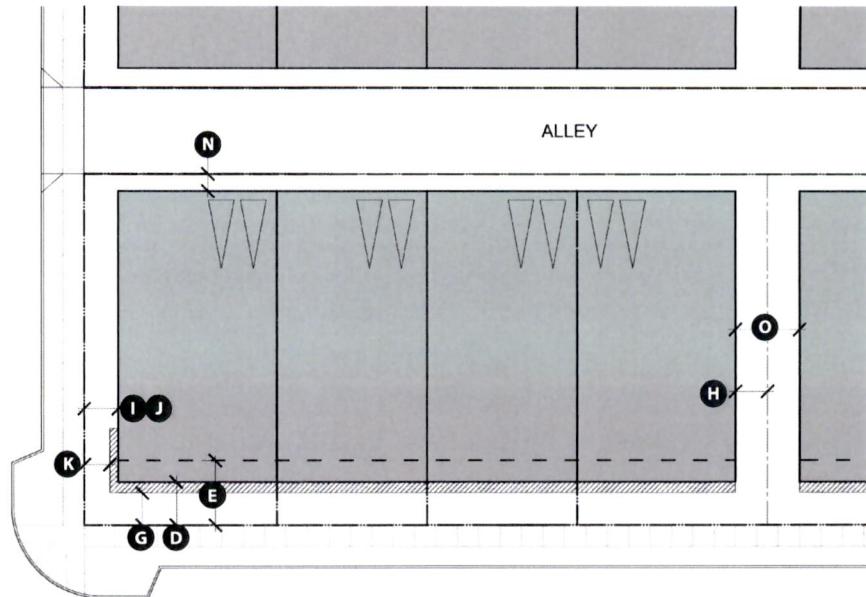
Side setbacks at internal lots



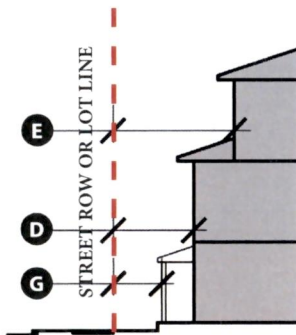
Side setbacks at corner



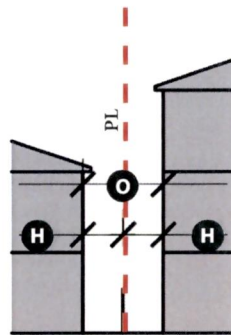
-  PORCH
-  BUILDABLE ENVELOPE LIMITS
-  PROPERTY LINE



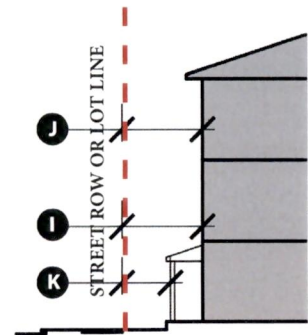
Medium Density Residential (MDR-16) - Attached, Alley Loaded



Front setbacks

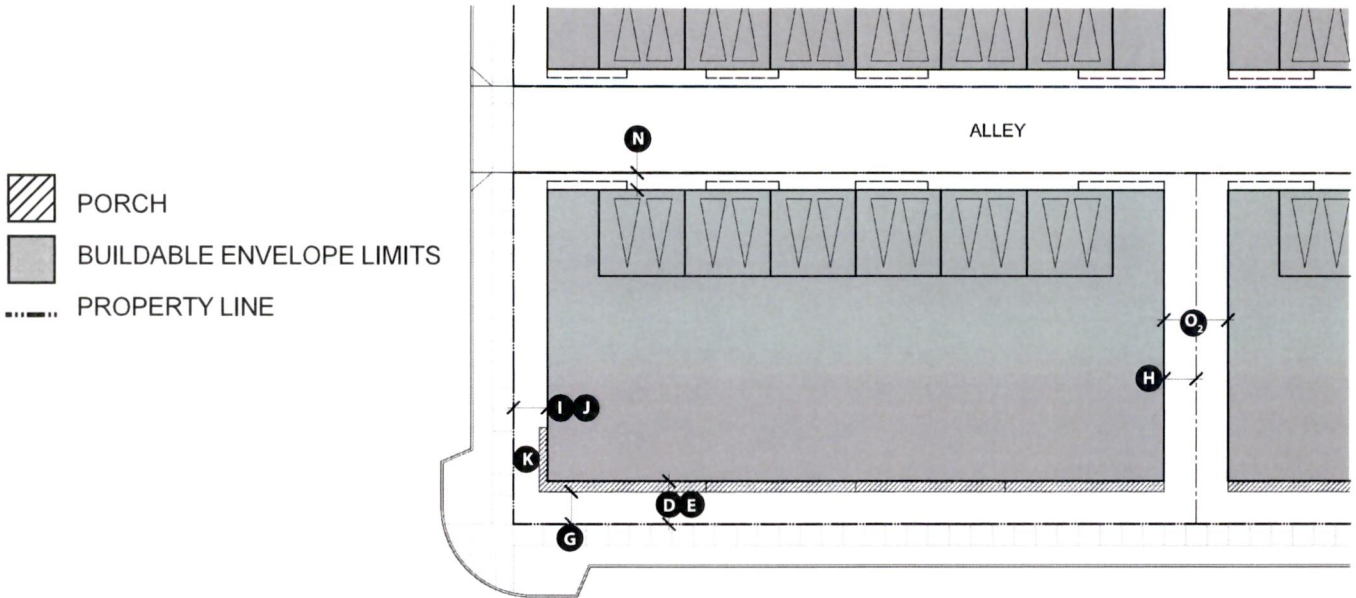


Side setbacks at internal lots

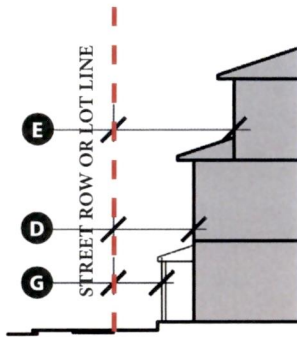


Side setbacks at corner

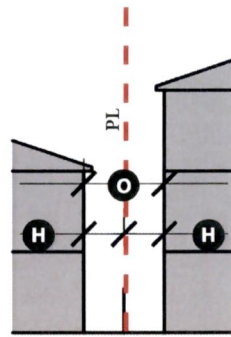




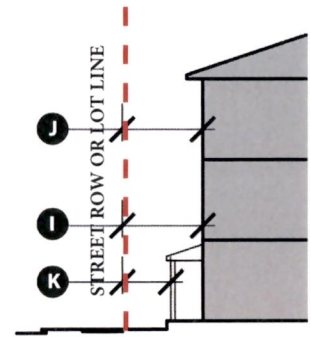
High Density Residential (HDR) - Alley Loaded



Front setbacks



Side setbacks at internal lots



Side setbacks at corner



both people and automobiles if not given proper attention. Therefore, planning in the early stages of development must consider the placement of service facilities and their effects on the public environment so that they do not create unsightly conditions. In order to minimize the negative visual and physical impacts of service facilities, the following treatments shall be applied:

- Trash storage, loading, and truck parking shall be located to minimize visibility from streets, pedestrian ways and building entrances and minimize interference with commercial or retail activities. Service and loading areas shall not be located along important pedestrian or view corridors.
- Service entrances shall not face primary or secondary retail and commercial streets. All service entrances and associated loading docks and storage areas shall be located to the side or rear of a building.
- Loading docks and truck parking shall be screened from public view using building mass, freestanding walls, and/or landscaping. Structural screening shall be made of materials and colors consistent with that of the principal structure(s).
- Roll-up security doors at service entrances shall be detailed to conceal door housings and tracks and provide an attractive, finished appearance for all exposed components.

All exterior trash receptacles shall be screened from public view on three sides, and on the fourth shall be screened by a gate that also obscures views. The enclosure shall be made of materials and colors compatible with that of the principal structure(s).

BUILDING LIGHTING

Moving, blinking, or strobe lights are prohibited.

Illuminating a franchise building as a form of advertising is prohibited.

6.4 Parking Regulations

6.4.1 PARKING REQUIREMENTS

REQUIRED OFF-STREET PARKING SPACES

Each land use identified below shall be provided at least the number of off-street parking spaces listed. Uses not listed are subject to the parking requirements of Chapter 19.21 of the City of American Canyon Municipal Code.

NVRG area: 1 space for 400 sf of enclosed covered area, and 1 space for 200 sf of outdoor seating area. Overflow parking in parking orchards may be counted towards fulfilling parking requirements. A Parking Management Plan (PMP) shall be submitted as part of the NVRG Master Plan/ Design Permit (see section 6.4.2.1).

Hotel: 1 parking space per room plus 1 space for every 10 rooms.

Residential: Off-street parking requirements shall be in accordance with Chapter 19.21 of the City of American Canyon municipal Code.

RV parking is not allowed in driveways, front yards, or on streets.

MINIMUM DIMENSIONS, UNCOVERED PARKING

Where parking stalls are located adjacent to landscaped areas, the paved depth of such stalls may be decreased by two feet provided a vehicle overhang area exists. The vehicle overhang area may not encroach into a public sidewalk unless that sidewalk is widened by the depth of the overhang. Parking area parking spaces and drive aisle dimensions shall be as listed below.

Standard space: 9 ft x 18 ft.

Compact space: Up to 15% of the total number of required parking spaces may be designed as compact spaces with minimum dimensions of 8.5 ft x 16 ft.

Drive aisle widths for parking lots with perpendicular parking is 24' minimum.

MINIMUM DIMENSIONS, COVERED PARKING

Garages shall be constructed to meet the following minimum dimensions:

Single car: 12 ft x 20 ft

Two car standard: 20 ft x 20 ft

Two car tandem: 12 ft x 36 ft. (Tandem parking is allowed

6.5 Signage Regulations

These regulations are intended to result in functional, attractive signage incorporating a high level of design, graphics and efficient maintenance throughout the WRSP Area. All signs shall meet the Signage Guidelines in the WRSP. A master signage plan shall be prepared for the NVRG area as part of the first Design Permit entitlement.

6.5.1 PROHIBITED SIGNS

In addition to the signs prohibited pursuant to Section 19.23.060, Prohibited Signs, of the American Canyon Municipal Code, the following signs are prohibited.

- Decal signage on glazing.
- Internally illuminated awnings.
- Plastic-faced monument signs.
- Conventional plastic-faced box or cabinet signs.
- Formed plastic or injection molded plastic signs.
- Signs with luminous vacuum-formed letters.
- Cloth, paper, cardboard or foam signs or decals.
- Blinking, flashing, animated or moving signs.
- Advertising displayed on vehicles to attract attention to a specific business location or sale.

6.5.2 COMMERCIAL TENANT IDENTIFICATION AND STOREFRONT SIGNAGE

Tenant signs shall be limited to the identification of building tenants. Tenant signage must comply with the Design Guidelines contained herein and the master signage program prepared for the NVRG Mixed Use area.

6.5.3 LIVE-WORK SIGNAGE

The following signs are prohibited for live/work developments:

- Franchise signage
- Internally lit acrylic or plastic signs



Examples of creative signage appropriate at the Napa Valley Ruins & Gardens

6.7 Walls and Fencing Regulations

Fences, walls, and dense hedges shall comply with the following regulations:

- Front Yards: Fences and walls within a required front yard shall not exceed 36 inches in height.
- Corner Intersections: Fences and walls at a corner intersection may not exceed 30 inches within the Public Works Standard for a Controlled Area at an intersection.
- Alleys: Fences and walls located within five feet of the edge of paving of an alley shall not exceed 36 inches in height.
- Where private side yards are adjacent to a street, privacy fencing should only enclose the area that is consolidated into usable yard space; privacy fencing to enclose minimum side setbacks adjacent to a street is not permitted.
- Sidewalks. Fences and walls within three feet of a sidewalk shall not exceed 36 inches in height.
- Continuous vandal resistant, safety fencing shall be required along the railroad right of way to prevent pedestrian and bicycle access into the railroad right of way. Fencing shall be a minimum of 6 feet in height and shall be designed to comply with the Design Guidelines in Appendix A, Section A.4.3.1 and the "view fencing" in section A.7. Where homes are located closer than 50 feet to the railroad right of way, a solid fencing design constructed of masonry or metal must be utilized.

See Figure 6.2 for an illustration of wall and fencing regulations.

6.8 Utility Regulations

All utility transformers and pedestals shall be located below ground in vaults to minimize their appearance in the community.

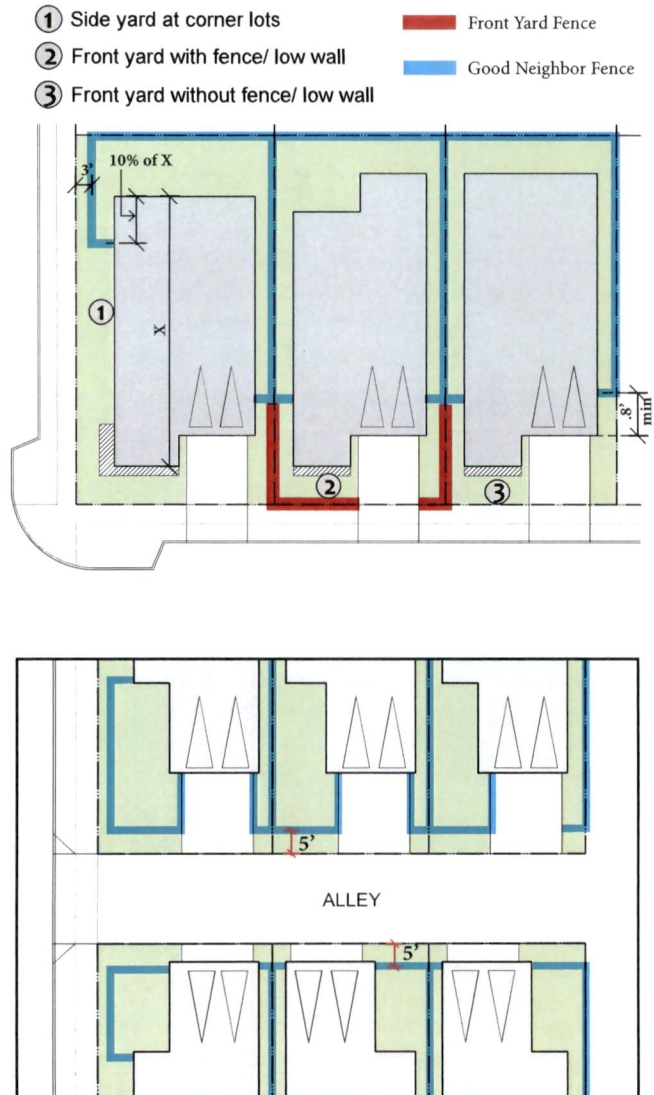


Figure 6.2: Fencing Regulations

- Residences and public/commercial buildings should be designed to accommodate electric vehicle charging stations.
- Incorporating signage on the sustainable design features on the build environment, specifically LEED certified buildings.
- Homebuilders providing buyers with a system option for the homes to include photovoltaic (solar) and solar water-heating systems. Being solar-ready may include:
 - Having a designated solar zone
 - Adequate electric capacity
 - Designated conduit and plumbing paths
- Commercial parking incorporating photovoltaic (solar) panels over the parking, or solar turbines located on the light poles to harvest energy.
- Low impact development principles should be implemented to reduce the impact of the development on the natural environment.
- Incorporate infiltration beds, swales, and basins into the design of a project to allow water to collect and soak into the ground, as required by local agency.
- Utilize pervious or porous surfaces (permeable pavers or blocks) to minimize runoff.
- Encourage the use of rain harvesting or catchment technologies (rain gardens, canisters, etc.)

TIER 3 DESIGN GUIDELINES

The third tier involves a commitment to sustainable landscape techniques. Projects shall incorporate The City of American Canyon Model Water Efficient Landscape Ordinance (MWELO) into the design (See Section 6.6.3 above). The desire to create community in accordance with green building principles influences the decisions regarding the landscape design, recreational programming, and physical layout of each development area. The sustainable landscape techniques that may be used include:

- Drought tolerant plants should be selected to reduce water consumption based upon:
 - Their adaptability to the natural conditions of the site.
 - Protection and preservation of native species and natural vegetation
 - Disease and pest resistance
 - Water-conserving plant and turf species
 - Selection of plants from the East Bay Municipal Water District's "Plants and Landscape for Summer-Dry Climates" as the primary reference.
- Plant materials should be selected that promote a diversity of flora and fauna.
- Recycled water for landscape should be considered to reduce the consumption and demand of potable water.
- Pursuant to Zoning Ordinance Chapter 19.22.040, the use of turf in new residential projects is limited to the rear yard.

CHAPTER 7 - CIRCULATION

7.1 Introduction

The Watson Ranch Specific Plan (WRSP), consistent with the City of American Canyon's General Plan Circulation Element, incorporates an array of transportation modes. This is achieved by including a network designed for pedestrians, bicyclists, and motor vehicles. Streets are not only functional systems that allow vehicles to navigate smoothly and safely within the community, but they are also an important building block in creating a rich and vital pedestrian environment. The Circulation Plan (Figure 7.1) provides access to residential, educational, commercial, and recreational destinations. In addition, the circulation plan promotes “complete streets”, a transportation policy and design approach that requires streets to be planned, designed, operated, and maintained to enable safe, convenient, and comfortable travel and access for users of all ages and abilities regardless of their mode of transportation. In doing so, the circulation plan provides a safe, functional, and pedestrian-friendly environment for residents and visitors of the WRSP Area community.

The WRSP Area is connected to the city on its western edge by Rio Del Mar East, which is proposed to be extended to serve as an east-west connector between Highway 29 and Newell Drive. From the southeast corner of the WRSP Area, Newell Drive is proposed to extend along the eastern edge of the WRSP Area, enabling a future northern connection of Newell Drive to SR 29 at Green Island Road. From a “roundabout” intersection connecting to the existing Newell Drive, the Newell Drive extension will continue to the north/northeast, while a small local road (Mallen Way) heads north connecting Rio Del Mar East and providing access to the hotel site.

In keeping with the General Plan goal of creating a well-connected community, the WRSP provides connections along its southern edge to Vintage Ranch. Respecting the existing development pattern of Vintage Ranch, connections are made to both existing Summerwood Drive and Rolling Hills Drive. The Rolling Hills Drive connection provides a connection to and from Vintage Ranch to Rio Del Mar East while Summerwood Drive connects the southern residential development parcels within the WRSP Area to Vintage Ranch. Complying with the American Canyon Fire Protection District, the WRSP Area will provide for two means of access during all phases of construction and at build-out.

The main circulation network within the WRSP Area is a modified grid with Rio Del Mar East forming the southern edge of the grid. The interconnected nature of the block pattern disperses traffic so that local streets are pleasant to live on and walk or bike along.

7.2 Goals, Objectives, and Policies

The following goals, objectives, and policies establish a framework to create a circulation network that considers the movement of pedestrians, bicyclists, and vehicles.

Goal 7A – Create a balanced transportation system that moves a variety of users through the community.

Objective 7.1 – Design pedestrian-friendly streets with appropriate traffic calming measures.

Policy 7.1.1 – Sidewalks within residential neighborhoods shall be a minimum of five feet wide.

Policy 7.1.2 – Standard crosswalks shall use accentuated markings, changes of material, pedestrian warning indicators, flashing bollards, flashing lane indicators and/or other flashing warning signs near parks, the NVRG, or major trail crossings.

Policy 7.1.3 – Bulb-outs shall be used at intersections subject to approval of the City of American Canyon and the American Canyon Fire Protection District.

Policy 7.1.4 – All streets, intersections, alleyways and other circulation improvements shall preserve public safety access throughout the WRSP Area.

Goal 7E – Design “Complete Streets.”

Objective 7.7 – Develop a street hierarchy and street system design that considers all potential users and establishes a Complete Streets network in the WRSP Area.

Policy 7.7.1 – Street design shall be consistent with Resolution 2012-72, “Complete Streets Policy of the City of American Canyon.”

Policy 7.7.2 – Class II bike lanes (delineated by on-street striping) shall be provided on all arterial and collector level streets throughout the WRSP Area except where Class I or Class IV bikeways are provided.

Policy 7.7.3 – Sidewalks shall be provided on at least one side of all streets.

Policy 7.7.4 – Approaches to signalized intersections should include bicycle detection devices that are operational and properly marked.

Policy 7.7.5 – Safety and access for bicyclists should be improved at at-grade railroad crossings by providing appropriate enhancements such as proper track structure, safe crossing angles, track fillers, lighting, and adequate warning and guidance information, among other features.

Policy 7.7.6 – Street design shall be consistent with the Circulation Element. Modifications not approved as part of this Specific Plan (see Section 7.3) shall be subject to review and approval through the tentative map and design permit approval process.

Goal 7F – Provide an open street network with connectivity to adjoining neighborhoods.

Objective 7.8 – Wherever possible, local street connections to adjoining neighborhoods should be made to provide local access to the Napa Valley Ruins and Gardens (NVRG) Area.

Policy 7.8.1 – Rolling Hills Drive shall be connected as a local street to Rio Del Mar East. Where necessary, traffic calming features shall be utilized to avoid “cut-through” traffic.

Policy 7.8.2 – Preserve the potential for the City to achieve a future at-grade crossing connection at the terminus of South Napa Junction Road until such time the City has completely abandoned the option as a secondary access into the NVRG.

7.3 Street Sections

This section describes the range of street classifications that make up the components of the overall circulation system. The Circulation Plan (Figure 7.1) delineates the location of the backbone street layout for the WRSP Area. The backbone circulation plan shows the general alignment of the primary streets. The locations of local streets and smaller streets will be determined at time of tentative map approval. Final street layouts may vary from street configurations illustrated below in this section, and may be determined at time of tentative map approval. The street sections described below include a series of “modified” street sections that are permissible to use within the WRSP Area.

In keeping with the sustainability goals of the WRSP, Low impact Development (LID) techniques may be used to reduce impacts of development on the natural environment. To meet this goal, planting strips or landscape medians may be used to treat stormwater runoff. The use of parkway strips or medians to treat stormwater may help in meeting the City’s MS4 Permit requirements, which is the regulatory framework to address appropriate source control, site design, and stormwater treatment measures in new development and redevelopment projects to address pollutant discharges and prevent increases in runoff flows.

In the following Street Section Graphics certain text may be abbreviated to fit graphically. These are:

- FOC delineates Face of Curb Dimensions
- BOC delineates Back of Curb Dimensions
- ROW delineates the Right of Way

Street sections illustrated represent the range of proposed streets within the WRSP Area. Specific locations and dimensions may be further refined or modified through the tentative map process (See Policy 7.7.6).

7.3.1 TWO LANE MAJOR COLLECTOR (NEWELL DRIVE)

This major collector consists of two 12-foot-wide lanes of travel, a 14-foot-wide median, 5-foot-wide Class II bike lanes, 8-foot-wide parkways, and 8-foot-wide sidewalks.

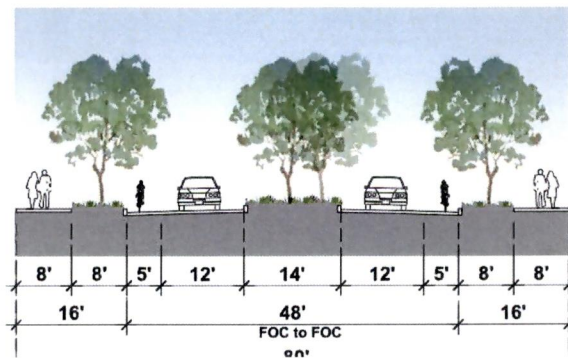
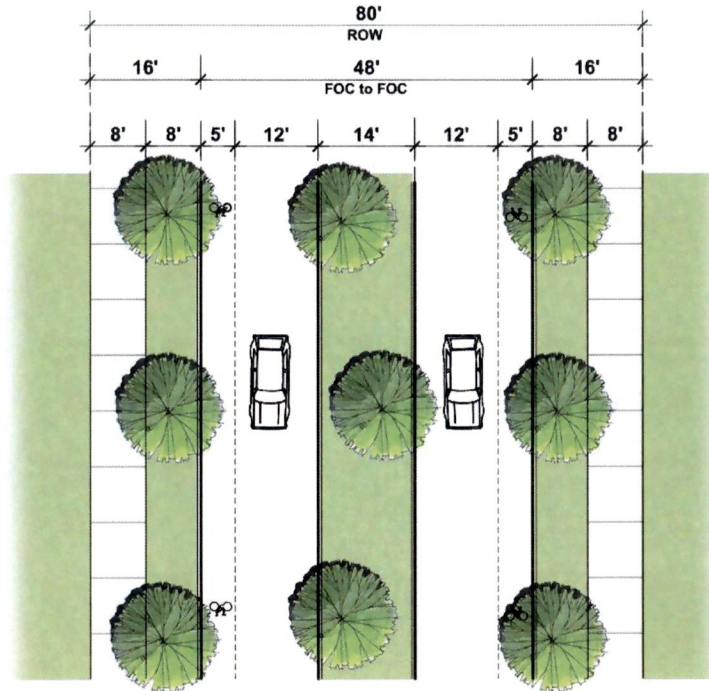


Figure 7.3.1: Two Lane Major Collector (Newell Drive)
Subject to modification

these figures represent typical street configurations; they are not indicative of the ultimate configuration of approved or future-approved modified major collector roadways such as Rio Del Mar East.)

Along the western edge of the site, Rio Del Mar East crosses the existing railroad. The Rio Del Mar East railroad crossing will be an at-grade or grade-separated crossing. Parking bays are eliminated for this portion of the roadway. Along the western edge of the site where not adjacent to the NVRG, the diagonal parking will become parallel spaces.

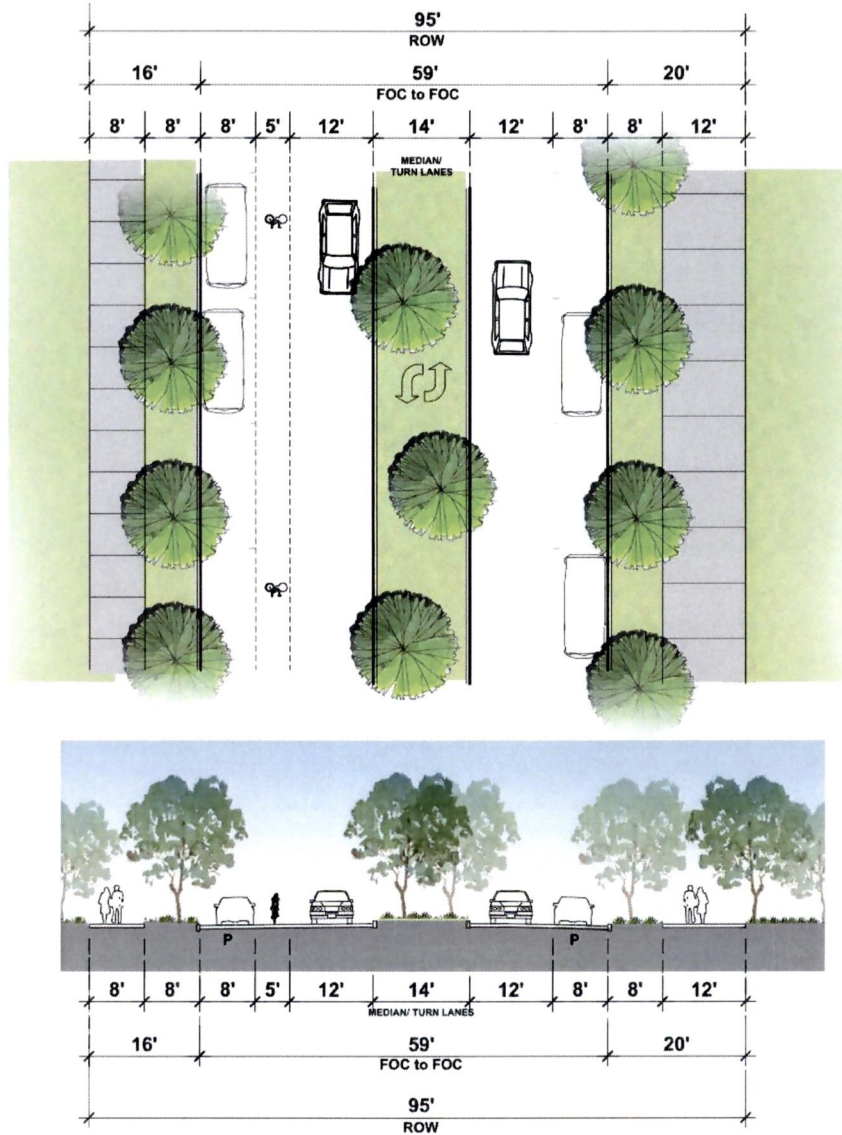


Figure 7.3.2b: Modified Collector (Rio Del Mar extension - from Napa Valley Vine Trail crossing to Newell Drive)
Subject to modification

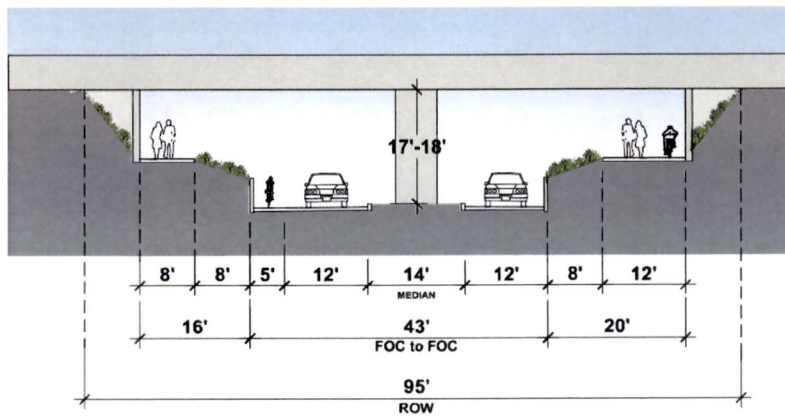
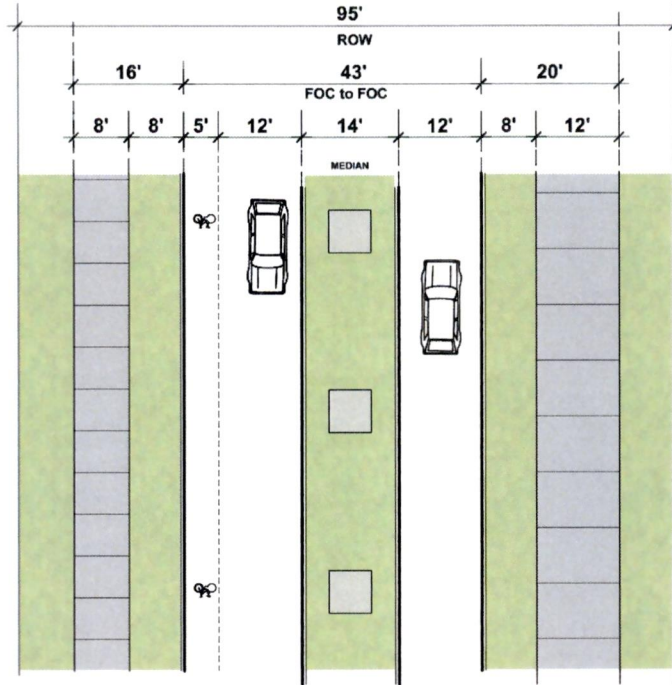


Figure 7.3.2d: Modified Collector (Rio Del Mar extension - RR underpass)
 Final railroad crossing is subject to C.P.U.C. approval and must meet C.P.U.C. regulations for vehicular, bicycle and pedestrian safety crossing features. Figure 7.3.2d is conceptual only and may be subject to change.

7.3.3 MINOR COLLECTOR

Marcus Road is a proposed two lane Minor Collector providing a primary connection through the project area. Marcus Road runs northward from Rio Del Mar East and then curves eastward to a terminus at Newell Drive.

Six-foot-wide sidewalks are provided on both sides of the minor collector and are each separated from travel lanes by a 6-foot-wide parkway strip. Where the parkway strip might be utilized for stormwater treatment, an increase in width may be necessary.

The standard Minor Collector street section consists of a 50-foot wide road section within a 74-foot wide right-of-way. The paved section will accommodate two 12-foot-wide travel lanes, 5-foot wide on-street bike lanes, and 8-foot-wide parking bays on either side. The roadway has a vertical curb and gutter and curb returns have a 20-foot radius.

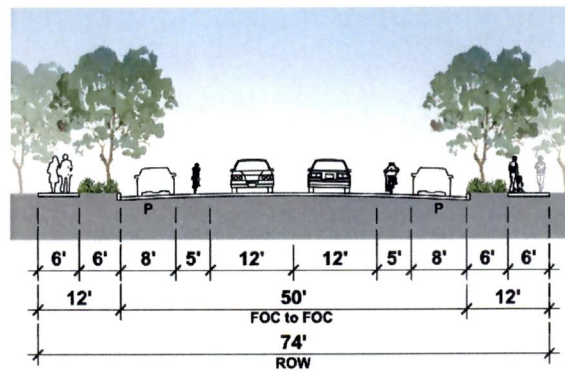
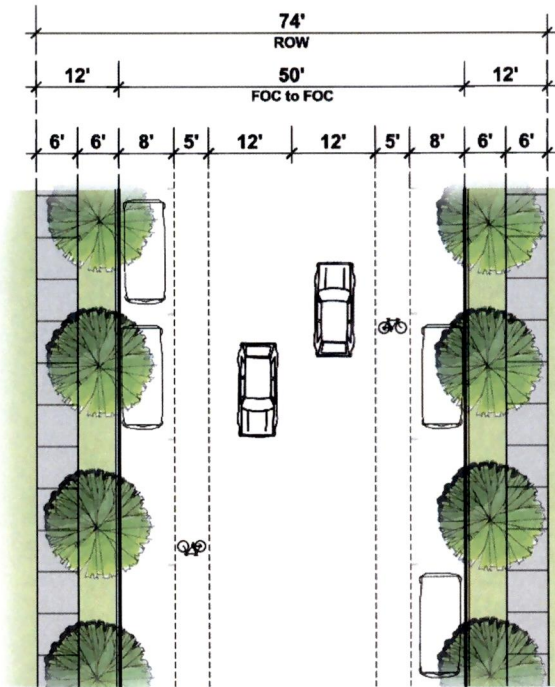


Figure 7.3.3: Minor Collector
Subject to modification

7.3.5 MODIFIED LOCAL STREET (RESIDENTIAL) - A

The Modified Local Street (Residential) - A is a street that is unique to the WRSP Area and provides access within residential neighborhoods. This street has a 36-foot-wide paved section within a 58-foot-wide right-of-way. The paved section accommodates two travel lanes with on-street parking on both sides.

The roadway has a vertical curb and gutter, and curb returns have a 20-foot radius. 5-foot-wide sidewalks are provided on both sides and are separated from the parking areas by a 6-foot-wide parkway strip.

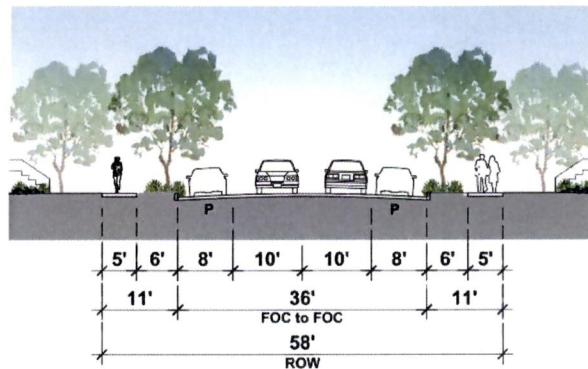
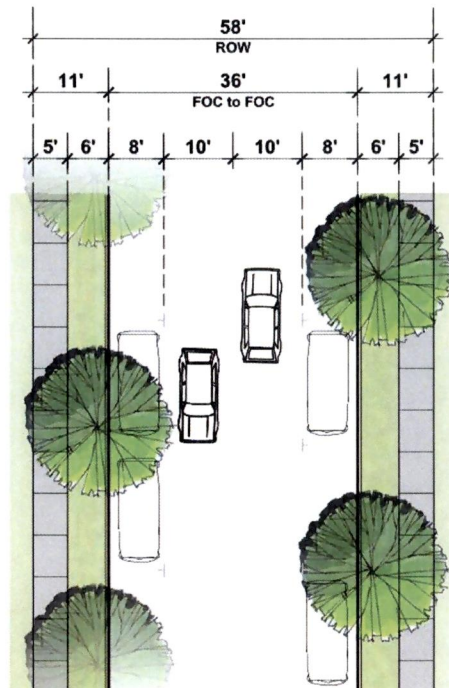


Figure 7.3.5: Modified Local Street (Residential) - A
Subject to modification

7.3.7 CUL-DE-SAC

The cul-de-sac is utilized at the end of a residential street primarily to serve as a turnaround.

The bulb of the cul-de-sac is located in a right-of-way which is 114-feet wide in diameter. The paved driving surface is 90-feet wide in diameter. The curb radius to the connecting neighborhood street is 17-feet. No parking is permitted within the cul-de-sac. A 6-foot-wide sidewalk that is separated by a 6-foot-wide planting strip is provided.

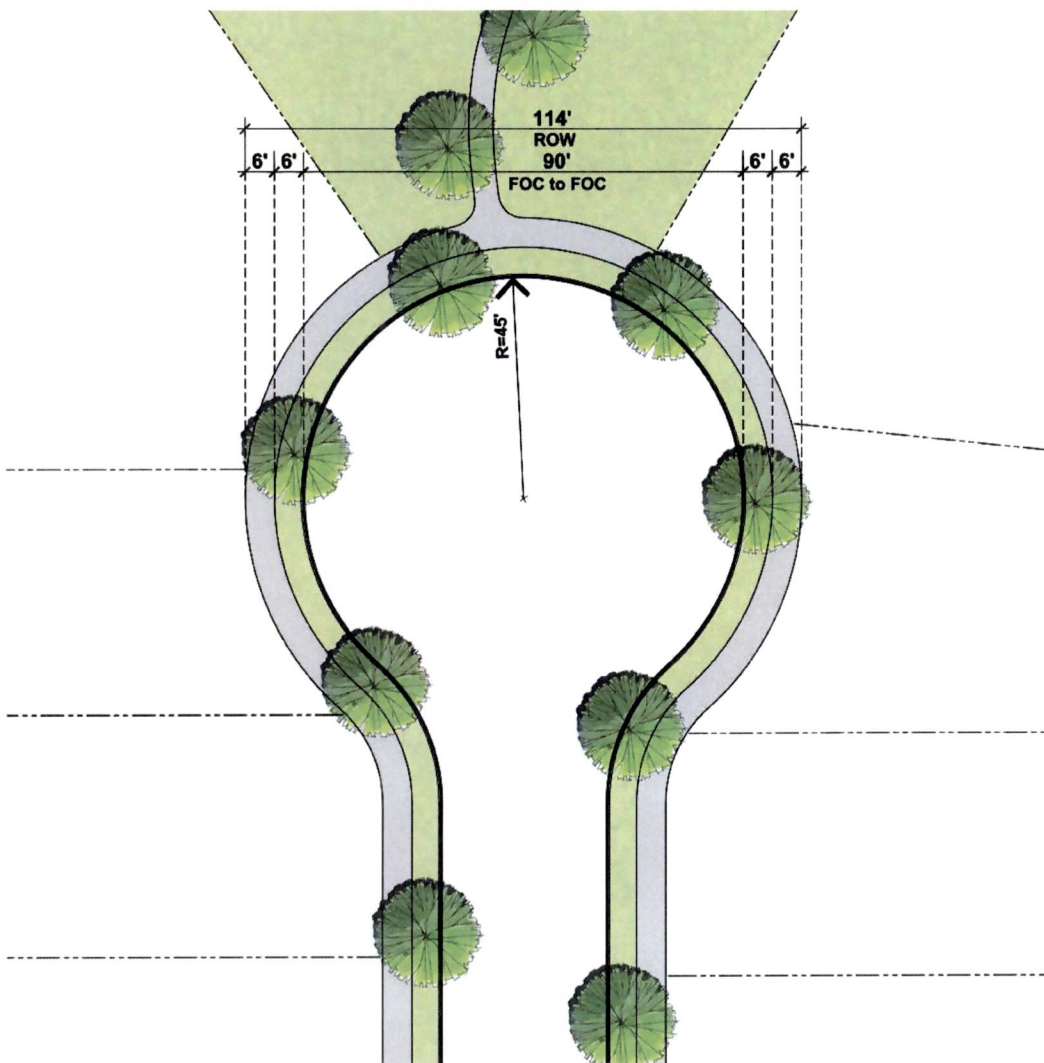


Figure 7.3.7: Cul-de-sac
Subject to modification

7.4 Traffic Calming

The Institute of Transportation Engineers defines traffic calming as “Traffic calming involves changes in street alignment, installation of barriers, and other physical measures to reduce traffic speeds and/or cut-through volumes, in the interest of street safety, livability, and other public purposes.” To promote walkability and to provide a safe pedestrian travel route within the community, various traffic calming measures could be adopted. The WRSP implements bulb-outs at intersections and narrowing at important pedestrian/vehicular intersection points, such as mid-block crossings for trail crossings, as shown in Figure 7.4.

In addition, the following traffic calming measures may also be considered, and their review and approval will be considered at tentative map review.

1. Vertical Deflection (Speed Humps; Speed Tables; Raised Intersections) Small changes in elevation can be used to slow vehicles. A speed hump is typically 12-14 feet in length and a height less than 4 inches, while a speed table is at least 22 feet in length rising up to 6 inches, with a flat central area at least 10 feet in length. Speed humps and tables work well in residential neighborhoods and are often used in series. Speed tables are sometimes used at midblock pedestrian crossings, with the crosswalk located across the flat area. Speed humps and speed tables are not used near intersections; rather, the entire intersection may be raised, with bollards to delineate sidewalks and pedestrian zones, making the entire intersection pedestrian-friendly. Special modifications must be made for drainage in the case of vertical deflections. Vertical deflections are not preferred on streets with significant (>8%) grade.
2. Horizontal Shifts (Traffic Circles; Chicanes) Traffic circles are raised islands installed at intersections of local or collector streets with one lane each direction entering the intersection; they should be adequately sized and configured to accommodate turning conditions for large vehicles such as buses or trucks. Chicanes are a series of at least three alternating curb extensions used at midblock locations only. Careful design must ensure that speeding cannot occur by drivers cutting straight paths across the center line. Chicanes can be created using on-street parking, but are often difficult to design due to driveway access. Horizontal shifts are sometimes preferred by emergency response teams over vertical deflections, and can provide opportunities for landscaping.

3. Other (Change in Material / Texture; Pedestrian Actuated Signals) Stamped asphalt, brick pavers, and rumble strips can be used to capture the attention of motorists. They are successfully used in conjunction with crosswalks or raised intersections / speed tables, and provide additional emphasis to mixed-use or commercial areas where there is also an increase in pedestrian activity. For especially dangerous geometries or high-volume / high-speed roads, pedestrian-actuated signals with flashers can be used at intersections or at mid-block locations.

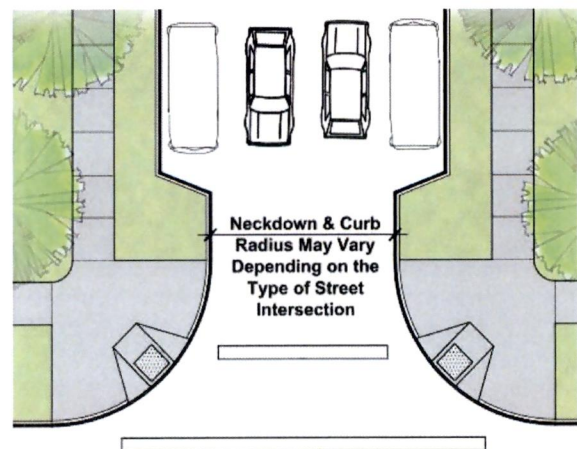


Figure 7.4: Intersection Neckdown/Bulb-out
Subject to modification

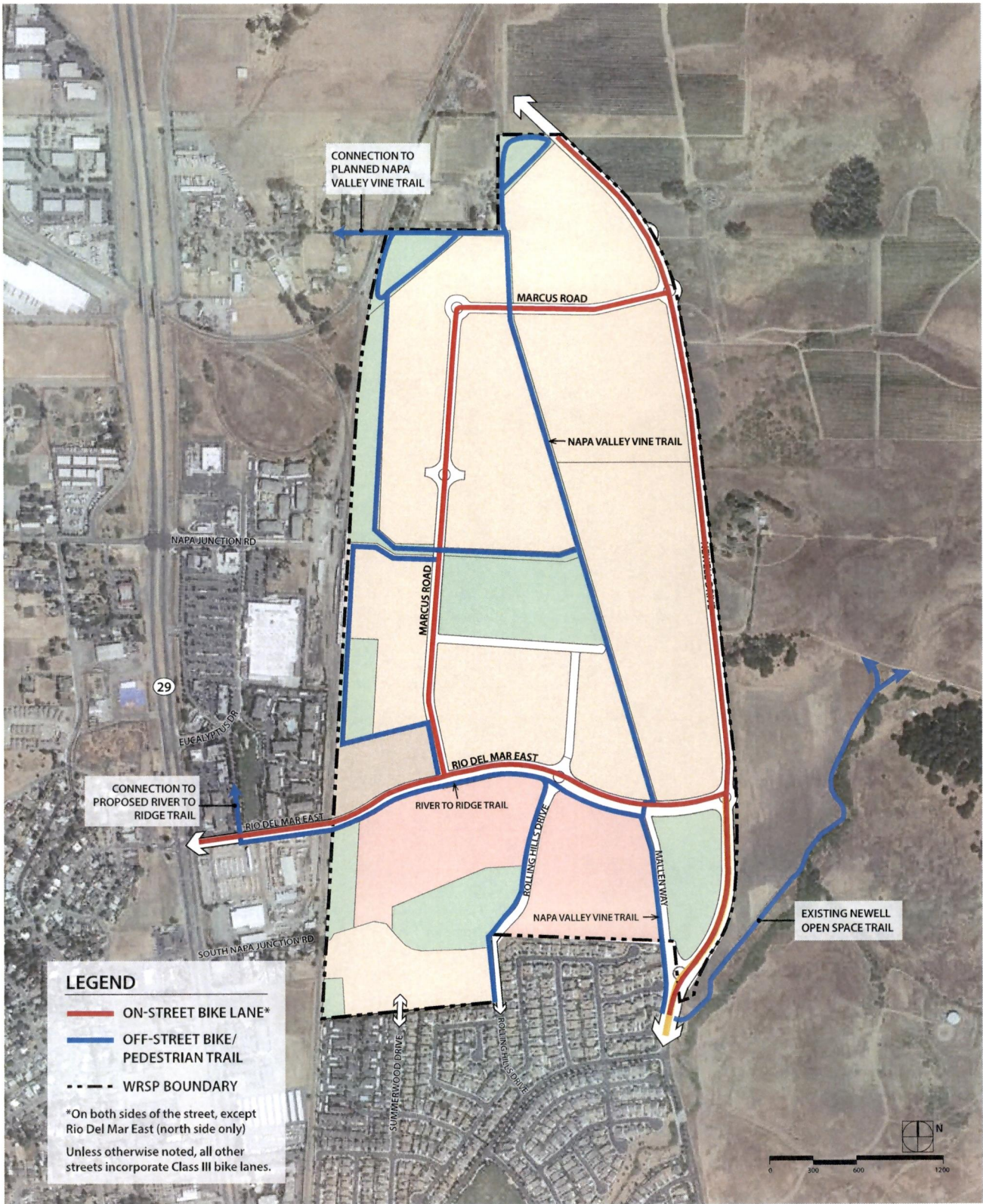


Figure 7.5: Pedestrian and Bicycle Circulation

7.6 Public Transit

American Canyon Transit (ACT) provides a deviated fixed route bus service aimed at getting local residents to shopping and healthcare facilities within American Canyon. ACT also connects to the Napa VINE Routes 11 and 29 allowing American Canyon residents and visitors to travel to Napa, Vallejo, the Vallejo Ferry and even BART in the East Bay. The VINE system serves all the cities in the Napa Valley and connects with SolTrans, Sonoma County Transit, Petaluma Transit, Lake County Transit, BART, the Vallejo Ferry, Amtrak, and Greyhound.

While the WRSP Area is not currently served by ACT, there is the potential for the extension of routes to include key destinations within the WRSP Area. Potential transit stops within the WRSP Area could be located in proximity to the NVRG, hotel site, Newell Park, and the Community Center. To preserve this potential opportunity, the major roads within the WRSP Area are based on the City of American Canyon's street standards and have sufficient lane widths and right-of-way to accommodate transit service and transit stops. Potential future routes could include extending existing bus routes or new routes through the WRSP Area on Rio Del Mar East and/or along Newell Drive. Figure 7.10 shows possible future bus routes and potential transit stop locations.

As noted in Section 1.3.4 of the WRSP, the WRSP Area is proximal to the Broadway District Specific Plan (BDSP) Area, which is a Priority Development Area (PDA) as designated by the Association of Bay Area Governments (ABAG) and Bay Area communities as areas for new growth. PDAs are considered foundational for sustainable regional growth and Plan Bay Area. PDAs emphasize housing near transit. The WRSP represents an extension of that growth being located less than a quarter mile from the SR 29 corridor. Although separated from the PDA by the Railroad line and commercial properties fronting SR 29, the WRSP provides a key point of access to the BDSP Area and SR 29 through the extension of Rio Del Mar East. This dramatically enhances connectivity to the primary transit corridor in American Canyon. Further, the potential transit stop shown at Rio Del Mar East and the NVRG frontage is a quarter mile from approximately 46% of the units (+/- 500) within the WRSP Area as well as the NVRG mixed uses and the hotel site.

7.7 Off-Site Improvements

Off-site improvements include major roadway connections from the WRSP Area to existing American Canyon roadways and several local street connections to enhance local connectivity. A connection to the WRSP Area at the existing Highway 29/Rio Del Mar East intersection will include surface improvements, roadway widening, and an at-grade or grade-separated railroad underpass to access the WRSP site.

The associated off-site improvements for the connection of Rio Del Mar East to SR 29/Broadway will include improvements to Rio Del Mar East from the western project boundary up to SR 29 and the intersection of Rio Del Mar East and SR 29 referred to as the "Tie-in" (see Figure 7.3.2e). Improvements at the Tie-in are intended to comply with California Department of Transportation (Caltrans) Encroachment Permit requirements, which may include the construction or financial contribution toward construction of the bicycle and pedestrian improvements at this intersection, identified in the Caltrans District 4 Bike Plan (2018) as NAP-29-X12. All improvements within the SR 29 State right-of-way requires an encroachment permit that is issued by Caltrans.

Newell Drive and Mallen Way will be connected to the south where Newell Drive currently terminates just north of Donaldson Way. Rolling Hills Drive will be connected to the south where it currently terminates just north of Terrazzo Lane.

The off-site improvements are shown on Figure 7.11.

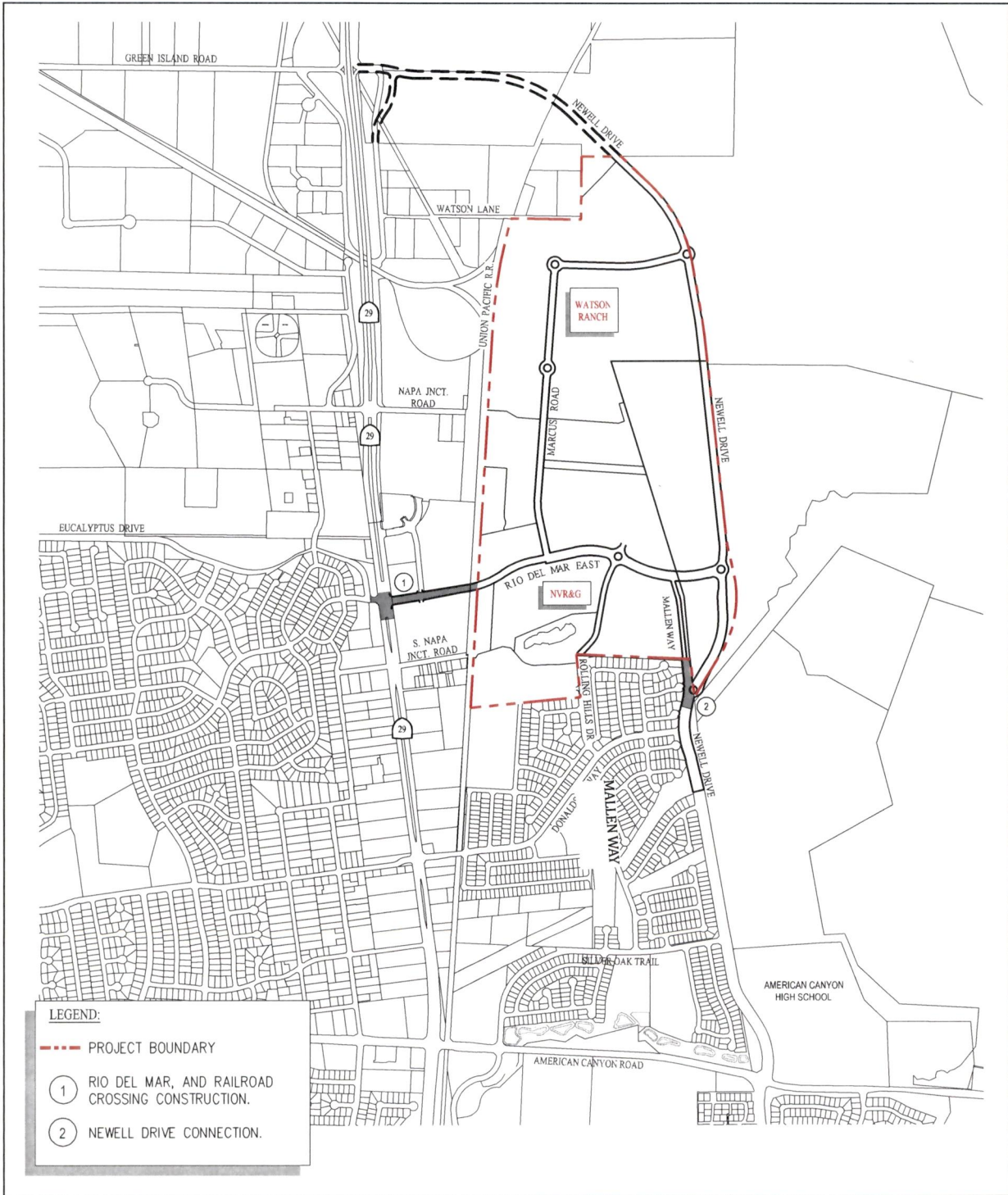


Figure 7.11: Summary of Off-Site Road Improvements

CHAPTER 8 - INFRASTRUCTURE AND PUBLIC SERVICES

8.1 Introduction

This chapter addresses the proposed infrastructure system and public services for the Watson Ranch Specific Plan (WRSP) Area. Infrastructure includes the water, wastewater and storm water runoff systems. The Public Services refer to public facilities and services and utilities necessary to serve the area. These include some citywide system improvements that will benefit more than the properties within the WRSP Area. The majority of infrastructure improvements will be privately funded. When warranted, reimbursement or assistance in financing may be provided when improvements exceed the project requirements and pro-rata share of responsibility for system improvements (see Implementation Chapter).

8.2 Goals, Objectives, and Policies

Goal 8A - To provide the infrastructure and public services required to support the WRSP Area without negatively affecting existing neighborhoods and residents of the City of American Canyon.

Objective 8.1- Fully comply with the City of American Canyon “Zero Water Footprint Policy” in the development of the water supply and system for the WRSP Area.

Policy 8.1.1 - New potable water sources shall be identified, or existing water supply lines or uses shall be retrofitted to reduce water consumption so that the proposed development shall comply with the Zero Water Footprint Policy.

Policy 8.1.2 - New development shall comply with the City of American Canyon’s Model Water Efficient Landscape Ordinance and with the new water rules established on May 2015.

Objective 8.2 - Address on-site storm water drainage and retention through Low Impact Development (L.I.D.) standards and compliance with BASMAA and City of American Canyon requirements.

Policy 8.2.1 - On-site detention basins shall be efficiently sized to accommodate storm water flows from the project without adversely affecting downstream capacities.

Objective 8.3 - Provide improvements necessary to ensure sufficient downstream capacity in wastewater system.

Policy 8.3.1 - The proposed wastewater system shall be designed and sized accordingly to meet the project needs without adversely affecting downstream capacities.

Objective 8.4 - Provide solid waste recycling opportunities to reduce dependence on landfilling of solid waste.

Policy 8.4.1 - The existing solid waste collection and recycling service that exists within the City of American Canyon shall be extended to serve the project area.

Objective 8.5 - Provide state of the art telecommunication and technology infrastructure (e.g., fiber – optic cable) throughout the WRSP Area.

Policy 8.5.1 - City and developer shall work with telecommunications and cable television providers to provide the most advanced, market ready infrastructure for telecommunications and television. Necessary infrastructure shall be installed in the WRSP Area to facilitate upgrades in service as the technology becomes available.

ON-SITE WATER IMPROVEMENTS

Proposed on-site backbone improvements include water lines ranging in size from 6-inch to 12-inch in diameter. The on-site improvements connect to the following infrastructure lines:

- Existing 12-inch water line (Zone 1) within Watson Lane.
- Existing water line (Zone 1) within the east side of Highway 29, at a location approximately 480 feet south of S Napa Junction Road.
- Existing 8-inch water line (Zone 1) within Summerwood Drive.
- Existing 8-inch water line (Zone 1) within Rolling Hills Drive.
- Existing 18-inch water line (Zone 3) within Newell Drive located north of the tank access road.

All of the facilities will be designed and constructed to meet the City of American Canyon Standards.

See Figure 8.2 – Water Infrastructure – On-Site Improvements.

8.3.2 SANITARY SEWER

A Sewer Flow Monitoring Capacity Study was prepared for the WRSP Area in August 2014 (V&A Report). Its findings presented two methods for estimating peak sewer flow demand. Using existing City criteria, the peak flow demand is estimated to be 1.28 mgd (million gallons per day). Using criteria that recognizes the reduced sewer flow that would occur as a result of the planned use of recycled water, the sewer flow demand is estimated to be 0.86 mgd. Both sewer flow results can be accommodated in the existing sewer main in American Canyon Road, if the existing sewer main in State Route 29 is upsized. The replacement and upsizing of the old sewer main in State Route 29, could significantly reduce the existing Inflow/Infiltration burden on the existing sewer system, and would offset most of the WRSP Area's increased burden on the City's wastewater treatment plant.

The City of American Canyon provides collection and treatment of the wastewater throughout the City. Wastewater is collected and directed to the Wastewater Treatment Plant on the east side of the City for treatment.

The following facilities are adjacent to the WRSP Area:

- 15-inch sanitary sewer line running behind the southern lots on South Napa Junction Road (east of State Route 29).
- 10-inch sanitary sewer line located along the west side of State Route 29.

- 6-inch sanitary sewer line running under State Route 29 that connects the 10" sanitary sewer (west side of State Route 29) to the 15-inch sanitary sewer (east side of State Route 29).
- 6-inch sanitary sewer line in Napa Junction Road.
- 8-inch sanitary sewer line in Summerwood Drive.

The WRSP Area connects to the existing 10" sanitary sewer in State Route 29. A portion of the WRSP Area also connects to the existing 8-inch sanitary sewer in Summerwood Drive. Based on existing flow information, pipe upsizing and replacement is necessary in State Route 29, replacing the existing 10" sewer line to 15" from approximately 500 feet south of South Napa Junction Road to American Canyon Road. The segment of this line from Donaldson Way to American Canyon Road is a City C.I.P. project, the cost of which to be borne by the C.I.P. A sanitary sewer lift station will be required on-site to help maintain a gravity sewer system throughout the WRSP Area.

Backbone on-site facilities will include gravity sewer lines ranging from 8 inches to 15 inches in size. Sewer facilities will be designed and constructed in accordance with City of American Canyon Standards.

See Figure 8.3 for the backbone sanitary sewer improvements.

8.3.3 RECYCLED WATER

It is estimated that the WRSP Area's use of recycled water will be approximately 250 acre-feet per year. The City currently has the capacity to produce 1,000 acre-feet per year of recycled water and given the current usage, there is sufficient capacity in the existing City recycled water treatment plant.

The City of American Canyon provides recycled water service. At present, limited recycled water facilities are located throughout the City and not all portions of the City contain recycled water infrastructure. There is a 12-inch recycled water line within Newell Drive just south of the WRSP Area that was installed with the Vintage Ranch development. This 12-inch mainline is served by a one-million-gallon recycled water tank located southeast of the site. A recycled water system in the WRSP Area will be tied in to the following facilities adjacent or proximate to the WRSP Area:

- 12-inch recycled water line in Newell Drive
- Recycled Water line in State Route 29
- 6-inch recycled water line in Watson Lane

As part of the on-site improvements, a 12-inch backbone recycled water system will be extended into the WRSP

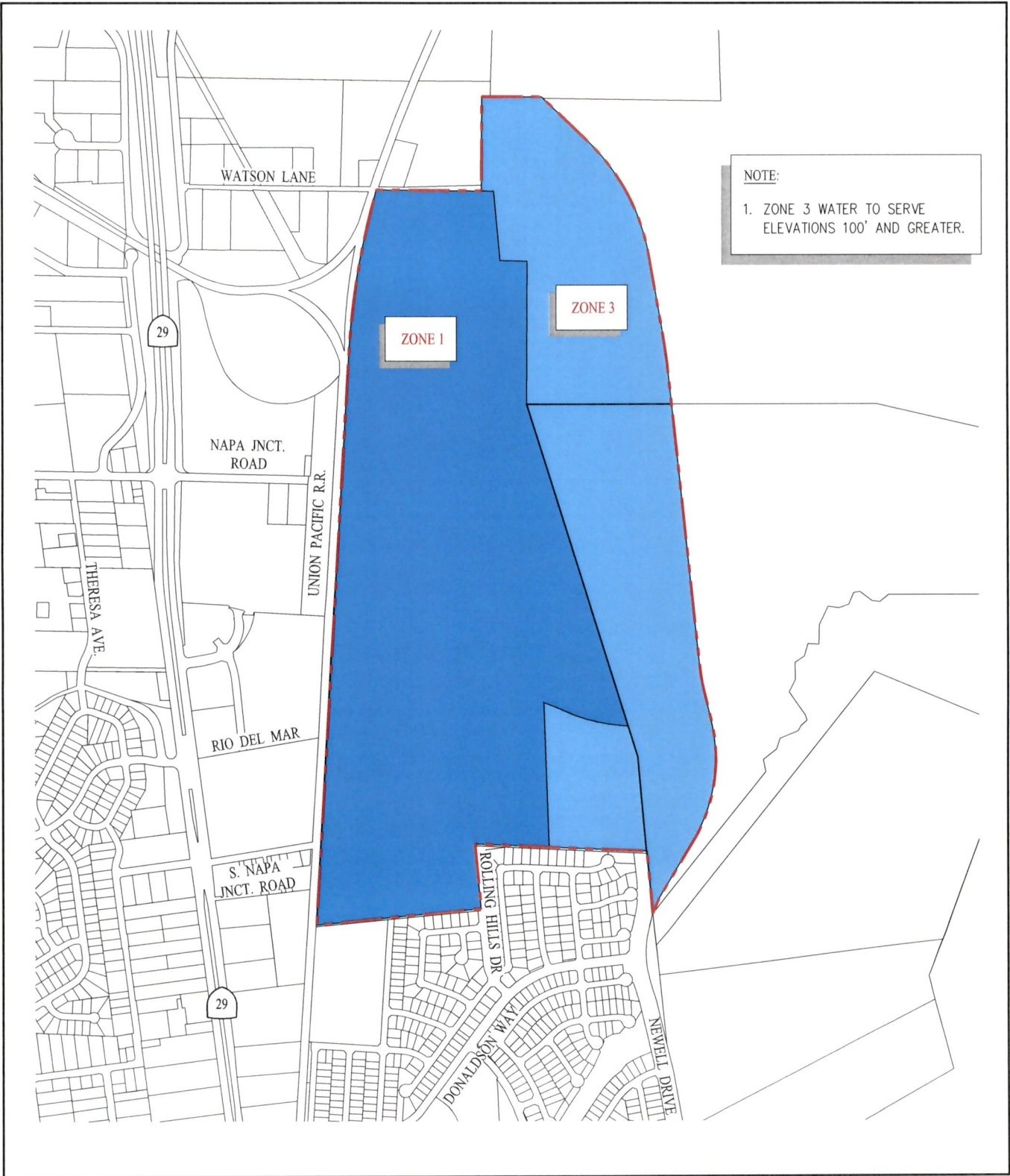


Figure 8.1: Water Service Zones

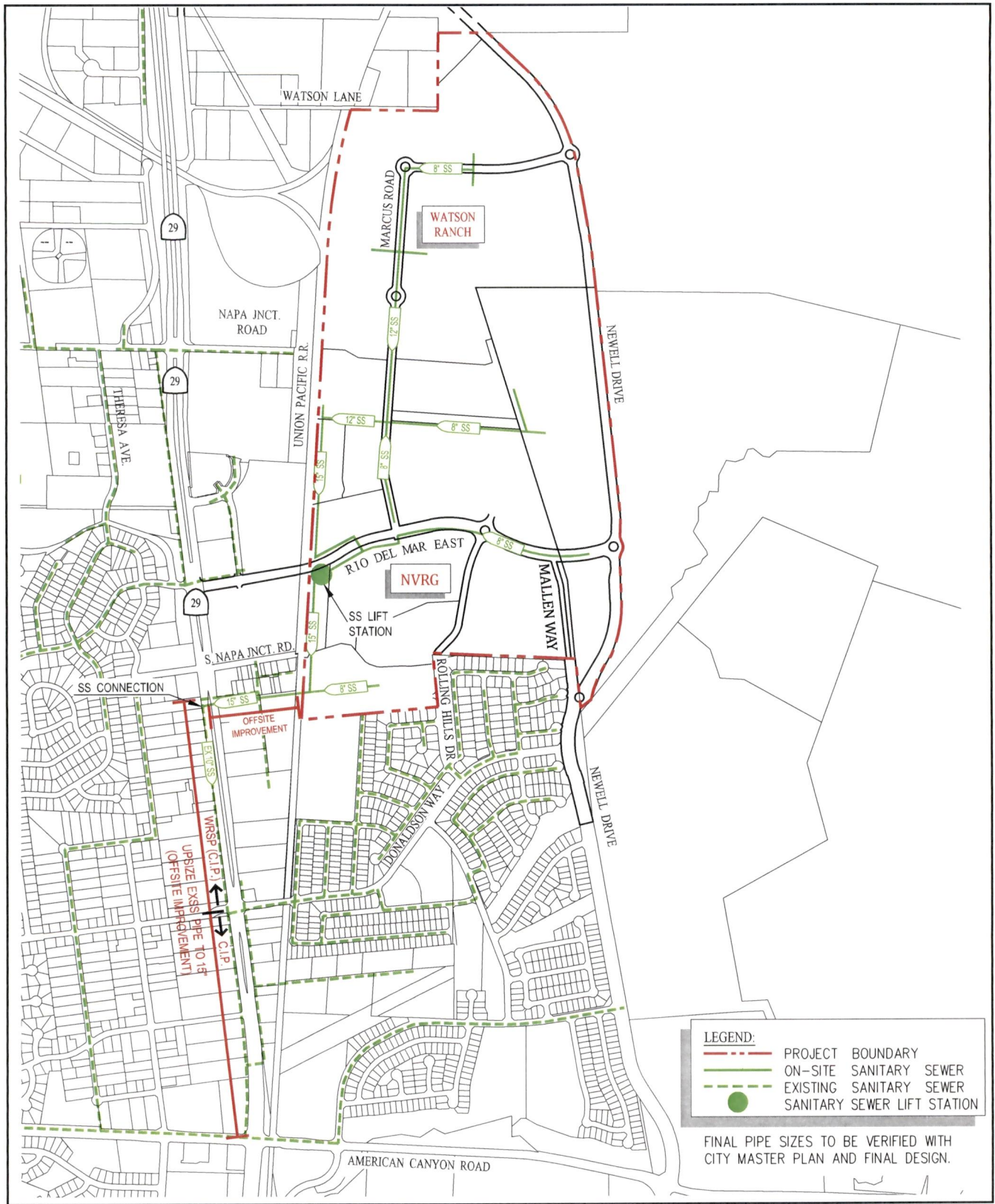


Figure 8.3: Sanitary Sewer Infrastructure Improvements

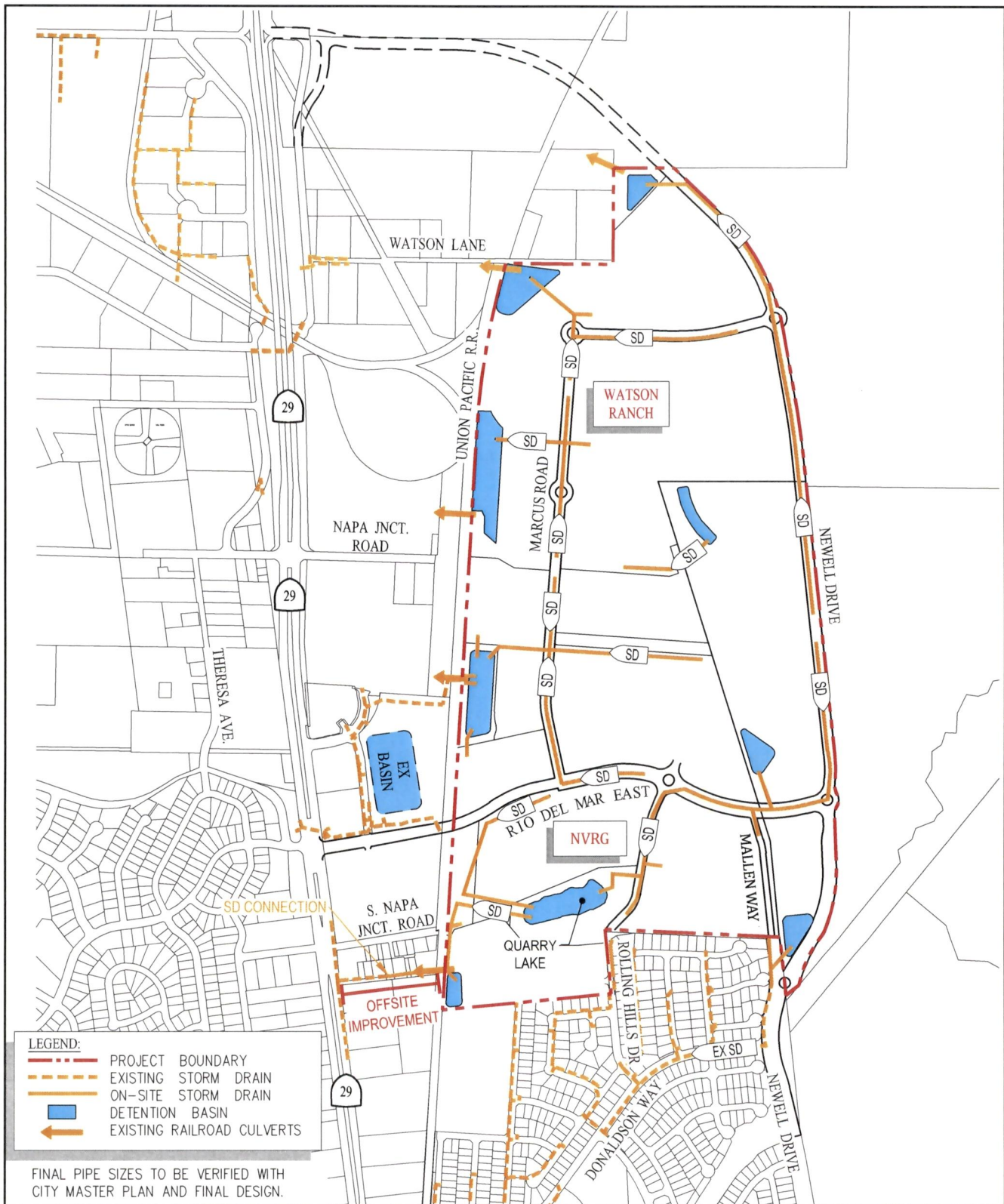


Figure 8.5: Storm Water Drainage Infrastructure

8.5 Off-Site Improvements

8.5.1 OFF-SITE DOMESTIC WATER IMPROVEMENTS

Off-site improvements will include the construction of a new Zone 3 (upper pressure zone) water tank to provide adequate supply, which is the responsibility of the City and funded by the C.I.P. This tank, along with a proposed Zone 1 tank have approved plans from the City. The Zone 1 tank (approximately 2.5 million gallons), which is not a part of the project, would be constructed next to the existing recycled water tank, north of the high school and southeast of the WRSP Area. The Zone 3 tank (2.0 million gallons), which is necessary to serve the portion of the WRSP generally east of the PG&E gas line alignment (see figure 8.1), will be constructed at a higher elevation to achieve required water pressure in a location southeast of the existing recycled water tank and Zone 1 tank site. The Zone 3 water tank will provide service to the upper pressure zone within the WRSP as well as other existing and future projects within the City. Zone 1 and Zone 3 water lines were installed from Newell Drive to the Recycled and the Zone 1 potable water tank site.

A 12" Zone 3 water line will be installed in Newell Drive from the southern boundary of the project to the existing 18" Zone 3 water line in Newell Drive located north of the tank access road. A 12" Zone 3 water line was also installed parallel to – and approximately 480 feet south of – South Napa Junction Road.

8.5.2 OFF-SITE SANITARY SEWER IMPROVEMENTS

The WRSP Area will upsize the existing 10" sanitary sewer in State Route 29 between South Napa Junction Road and American Canyon Road to a 15" sanitary sewer line. The segment of this line from Crawford Way to American Canyon Road is a City C.I.P. project, to be funded by the C.I.P.

8.5.3 OFF-SITE RECYCLED WATER IMPROVEMENTS

Off-site improvements include a 12" recycled water line south of – and running parallel to – South Napa Junction Road. A 12" recycled water line will be installed east of Watson Lane (east of the Railroad), connecting the WRSP Area to the existing 6" recycled water line in Watson Lane (ends at the Railroad), which is the responsibility of the City and funded by the C.I.P. Both of the WRSP Area's offsite recycled water connections will provide better redundancy in the existing City system.

8.5.4 OFF-SITE STORM DRAIN IMPROVEMENTS

Off-site improvements include a connection from detention basins in the southwest corner of the WRSP Area to the existing storm drain located just east of State Route 29 and south of South Napa Junction Road, which is the responsibility of the City and funded by the C.I.P.

CHAPTER 9 - IMPLEMENTATION

9.1 Introduction

This chapter fulfills the requirements of California Government Code Section 65451(a) (4), which requires that a Specific Plan include a program of implementation measures. This chapter shall serve as the program of implementation measures for the Watson Ranch Specific Plan (WRSP). It includes administrative procedures, public works projects, phasing and proposed methods of financing needed to carry out the WRSP.

The purpose of the implementation chapter is to ensure that:

- Public infrastructure and services are provided concurrently with private development.
- Adequate financing is in place to pay for the construction of necessary public facilities and improvements as defined in the Backbone Infrastructure.
- Adequate financing is in place to pay for the operation and maintenance of privately maintained facilities.
- Construction of public infrastructure by the city is coordinated with that constructed by the development.
- Consistency is achieved between the provisions of the WRSP and Development Agreement(s).
- Administrative procedures are in place to regulate development, in accordance with the standards and policies of the WRSP.
- Appropriate flexibility is provided to allow the project to respond to changing market conditions.

9.3.2 INFRASTRUCTURE PHASING

Infrastructure shall be constructed to serve development with the WRSP Area. As such, infrastructure planning, development, construction, completion, operation and maintenance shall be commensurate with development within the WRSP Area.

Infrastructure improvements are described in Chapter 8.

The following maps (Figures 9.1 through 9.5) depict anticipated phasing of Backbone Infrastructure for the WRSP Area. These figures are for illustrative purposes only to indicate the general progression of infrastructure improvements anticipated through project completion. Actual phasing will be established in the adopted Development Agreement(s).

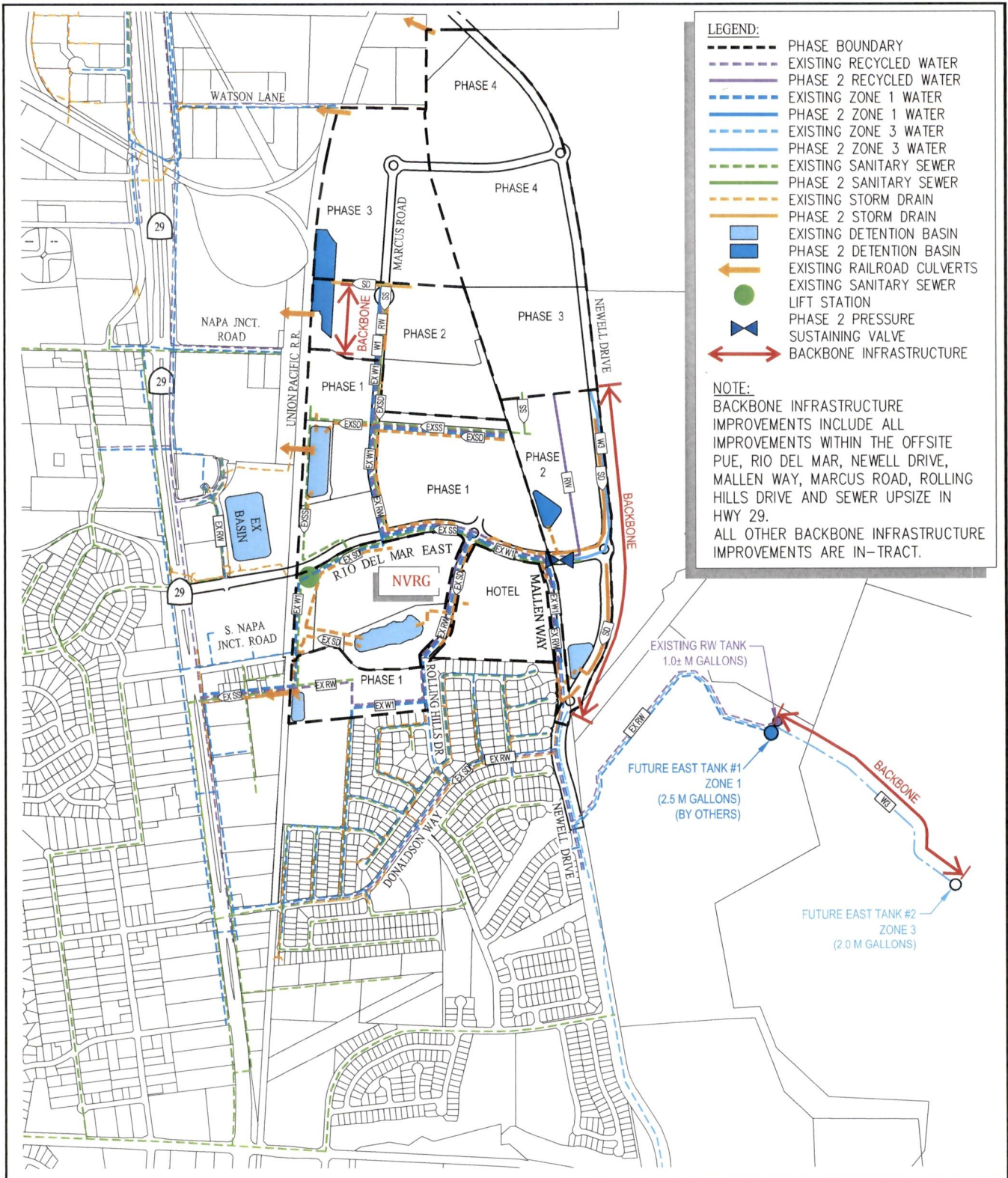


Figure 9.2: Phase 2 - Potential Infrastructure Improvements - Subject to Modification

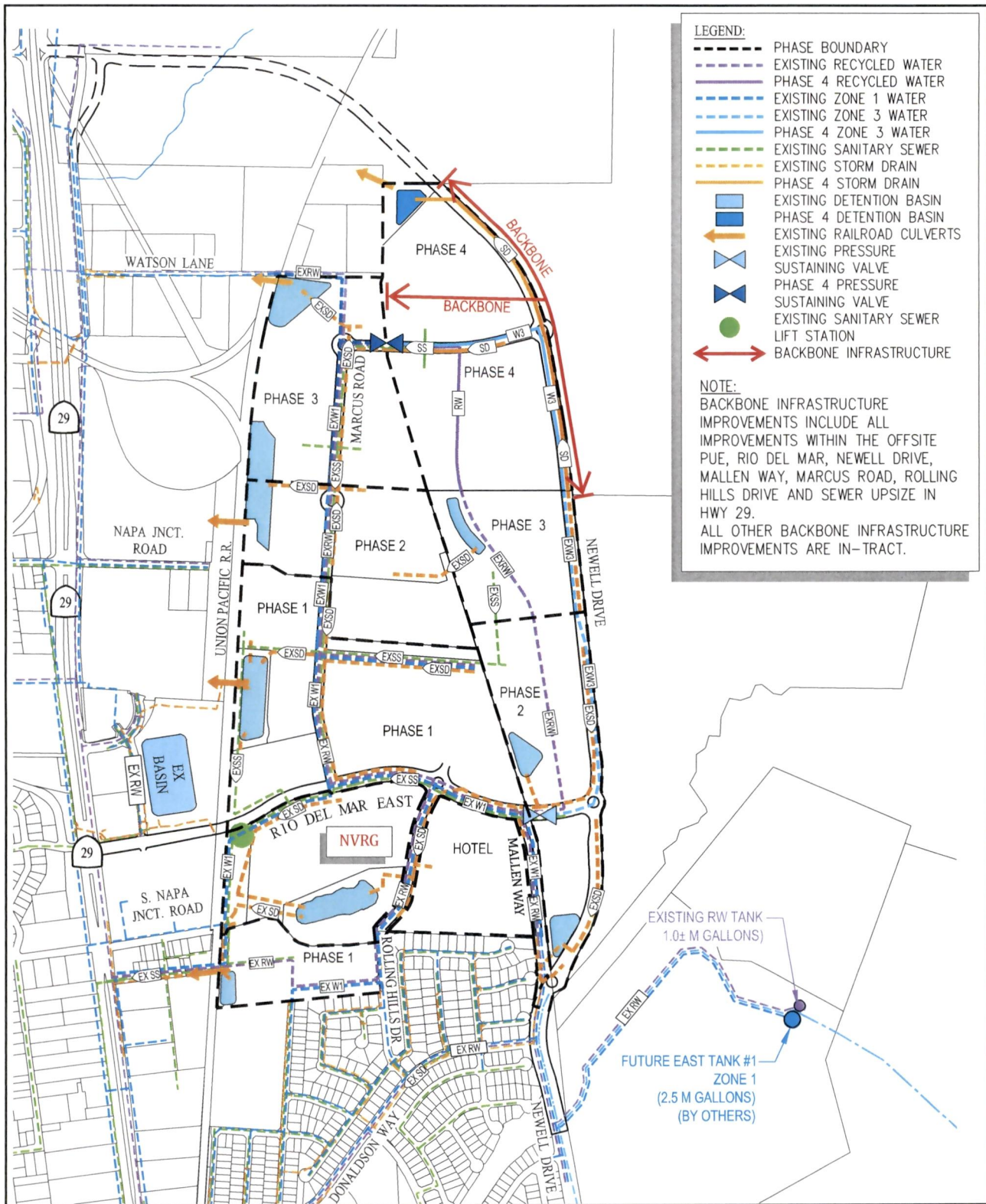


Figure 9.4: Phase 4 - Potential Infrastructure Improvements - Subject to Modification

9.3.3 PHASING OF LAND DEDICATIONS

1. General

All land necessary for public improvements and facilities as required by the WRSP, shall be offered for dedication to the City of American Canyon in accordance with the timeframes generally described below and more specifically defined in the Development Agreement(s).

All land subject to dedication shall be offered for dedication at no cost to the City, unless otherwise agreed to by City and Owner and shall be free and clear of all liens and encumbrances that would preclude its intended use.

Land dedication requirements of any subdivision map or parcel map approved within the WRSP Area shall not exceed the standards of the Specific Plan or accompanying Development Agreement(s).

Certain required elements of the WRSP's public infrastructure is located outside of the Plan's boundaries, and on lands not owned by the property owner. The general location of such infrastructure elements are indicated in graphics in Chapter 7, Circulation, and Chapter 8, Infrastructure and Public Facilities. The City and the WRSP developers shall work collaboratively to secure such sites and rights-of-way necessary for critical backbone infrastructure.

2. Land Dedication Conditions for First Final Map:

- Newell Drive as described in the approved Development Agreement(s)
- Rio Del Mar East from Newell Drive to the Western Project Boundary
- Associated road rights of way and utilities as described in the approved Development Agreement(s)
- Associated Parks and Open Space as described in the approved Development Agreement(s)

3. Land Dedication Conditions of Subsequent Final Maps

- Associated road rights of way, utilities and associated Parks trails and open space areas as described in the approved Development Agreement(s)

9.3.4 PHASING OF PUBLIC AND QUASI-PUBLIC INFRASTRUCTURE

The public and quasi-public infrastructure in the WRSP Area is comprised of the streets, parks, open space, and trails. The Parks and Open Space areas will be provided in conjunction with the appropriate subdivision map and may include dedication with first subdivision map

and construction of subdivision improvements. Specific conditions and timing of improvements shall be described in the Development Agreement(s).

9.3.5 PHASING OF PRIVATE DEVELOPMENT

General Policies Regarding the Phasing of Private Development

1. NVRG is a development priority in the WRSP. Development of the NVRG may commence pursuant to the phasing plan as part of phase 1. If not initiated during phase 1 of the residential development, the NVRG must commence development in conjunction with the start of phase 2 of the residential development.
2. NVRG Property owner shall have the right to commence development of the NVRG at any time provided Backbone Infrastructure (as designated in the Phasing Plan) has been approved and is under construction.
3. Private development in a subsequent phase of residential development may commence before development of a previous phase is complete provided Infrastructure Backbone (as designated in the Phasing Plan) has been approved and is under construction.
4. Phase boundaries may be modified if necessary as development progresses, subject to the approval of the City and general consistency with the proposed phasing plan.
5. Multiple final maps may be filed with each phase of development when approved as part of the tentative map entitlement stage.
6. Dedication and construction (or the requirement to cause the finance and construction) of Rolling Hills Drive shall occur at the commencement of Phase 2 of the residential development.
7. Phase 1 of the WRSP residential development may commence without the completion of the Rio Del Mar East extension (off site improvement) subject to the approval of at least two points of access to the Phase 1 area. No more than 743 homes may be constructed without completion of the Rio Del Mar East railroad crossing and the connection to SR29. The 186 affordable housing units do not count towards this limit.
8. The Railroad crossing at Rio Del Mar East shall first be pursued for PUC approval as an at grade crossing. Should the at grade crossing not be approved, the grade separated crossing will be pursued.

9.4.2 INFRASTRUCTURE AND IMPROVEMENTS

The infrastructure requirements for the WRSP Area are composed of a variety of backbone infrastructure projects including land dedications, roadways, water supply improvements, and sewer infrastructure. The WRSP Area also includes a variety of other public facilities that have community benefits beyond the WRSP Area.

9.4.3 FINANCING PRINCIPLES AND POLICIES

The following principles and related policies will govern the funding of infrastructure and public facilities for the WRSP Area. The principles will guide future decisions regarding formation of financing entities, adoption of financing mechanisms, and project approvals.

Note: The approved Development Agreement between AC-1 and the City of American Canyon establishes precise direction implementing the WRSP principles and policies. In any areas of conflict, the terms and conditions of the Development Agreement shall control.

1. A financial analysis and funding strategy shall be prepared that identifies the private and public infrastructure and facilities (improvements) required for the WRSP Area, the allocation of these costs, and proposed methods for funding.

Policy 1.1: The financial analysis and funding strategy shall take into account the phasing and build out of the proposed development and all improvements needed to accomplish the development and their respective costs, and it should provide a financial analysis of all funding mechanisms proposed.

2. The WRSP shall not be a burden to existing residents and will cover the pro rata cost for development associated with the build out of the WRSP Area.

Policy 2.1: Funding mechanisms shall be established such that new development and property owners within the WRSP and in the City fund the infrastructure and facilities required to provide needed public services and utilities to the WRSP Area.

Policy 2.2: Existing residents within or nearby the WRSP Area should not be burdened with assessments or taxes to pay for new public facilities if no benefit is received. Any new assessments or taxes should be directly related to the benefit received and not place an undue burden on these residents.

Policy 2.3: City development impact fees should be reevaluated to determine the development impact

fee amount that new development outside the WRSP Area should have to pay toward infrastructure improvements and facilities included within the WRSP Area. Additionally, a partial or full impact fee credit may be granted for public infrastructure and facilities that are provided as part of new development in the WRSP Area (as further described in Policy 7 below).

Policy 2.4: Properties outside the WRSP Area that benefit from public improvements provided by the WRSP should contribute their fair share to public improvement funding.

3. Future development within the WRSP Area shall pay the costs of mitigating impacts of the WRSP development on existing facilities and infrastructure in other parts of the City as identified in the WRSP FEIR and Mitigation Monitoring Program and as set forth in the Development Agreement(s). Future development within the WRSP Area shall not pay the cost of addressing existing deficiencies in parks and all other public improvements in the City of American Canyon.

Policy 3.1: WRSP Area development shall mitigate offsite public infrastructure and facility impacts as specified in the WRSP Environmental Impact Report. WRSP Area development shall pay existing impact fees, help fund public improvements, or undertake needed public improvements in order to mitigate impacts on existing facilities and infrastructure in other parts of the City as defined in and determined by approved Development Agreement(s).

4. Infrastructure costs shall be allocated among WRSP Area and surrounding properties based on the principle of benefit received (fair share allocation or rational nexus).

Policy 4.1: The City shall establish, on the basis of the financial analysis, a fair-share cost allocation for required public improvements to be borne by all benefiting new development within and adjoining the WRSP Area. These public improvement cost obligations shall be funded by developer construction and dedication of improvements, as an area development impact fee, and/or included in a local land-secured financing district covering the WRSP Area.

Policy 4.2: The City shall identify land (e.g., right of way) requirements for public improvements and establish a "fair share" (average) allocation to all developers and property owners in the WRSP Area.

5. Total public infrastructure and financing costs in the WRSP Area should not exceed industry standards of financial feasibility or benefit/cost burden on property owners.

WATSON RANCH SPECIFIC PLAN AREA FEES, DEDICATIONS, AND EXACTIONS

WATSON RANCH SPECIFIC PLAN IMPACT FEES

WRSP impact fees may be enacted by a legislative body (i.e., city or county) through adoption of an ordinance. Such fees, if adopted prior to the development of the WRSP, do not require a public vote to be enacted, but they do require public hearings. WRSP fees must be directly related to the benefits received. They do not create a lien against property, but must be paid in full as a condition of approval. The principal use of these fees is to construct backbone infrastructure and facilities essential to the development of the WRSP Area that is not otherwise part of the City-wide impact fee program or other City funding source. WRSP fees are established so that all properties within the WRSP Area pay their fair share at the time they are developed. The WRSP fee effectively replaces a myriad of other funding devices (e.g. Area of benefit assessments, etc.). Proceeds may be used to reimburse property owners who pay up-front costs for facilities benefiting other properties.

DEDICATIONS AND EXACTIONS

Under the Subdivision Map Act, developers may be required to dedicate land or make cash payments for public facilities required or affected by their project (e.g., road right-of-way). Dedications are typically made for road and utility rights-of-way, park sites, and land for other public facilities. Cash contributions are made for other public facilities that are directly required by their projects (e.g., payments for a traffic signal).

LAND SECURED FINANCING

SPECIAL ASSESSMENT DISTRICTS (1911, 1913, 1915 ACTS)

California law provides procedures to levy assessments against benefiting properties and issue tax-exempt bonds to finance public facilities and infrastructure improvements. Assessment districts, also known as improvement districts, are initiated by the legislative body (e.g., city council), subject to majority protest of property owners. Assessments are distributed in proportion to the benefits received by each property as determined by engineering analysis, and form a lien against property. Special assessments are fixed dollar amounts and may be prepaid, although they are typically paid back with interest over time by the assessed property owner. Only improvements with property-specific benefits (e.g., roads, and sewer and water improvements) may be financed with assessments.

MELLO-ROOS COMMUNITY FACILITIES DISTRICTS

California's Mello-Roos Community Facilities Act of 1982 allows for the creation of a special district authorized to levy a special tax and issue tax exempt bonds to finance public infrastructure, facilities and services. A CFD may be initiated by the legislative body or by property owner petition, and must be approved by a two-thirds majority of either property owners or registered voters (if there are more than 12 registered voters living in the area). Special taxes are collected annually with property taxes, and may be prepaid if prepayment provisions are specified in the tax formula. The special tax amount is based upon a special tax lien against the property. There is no requirement that the tax be apportioned on the basis of direct benefit. Because there is no requirement to show direct benefit, Mello-Roos levies may be used to fund improvements of general benefit, such as schools, fire and police facilities, libraries and parks, open space, detention basins, etc. as well as improvements that benefit specific properties. The provision also allows for the allocation of cost burdens to alleviate burdens on specific classes of development.

ENHANCED INFRASTRUCTURE FINANCING DISTRICT

The Enhanced Infrastructure Financing District (EIFD) is a new financing tool (see Government Code Section 53398.50 et, seq.) providing broad authority to local agencies to use tax increment to finance a wide variety of projects ranging from infrastructure such as roads, bridges and wastewater facilities to parks and open space, affordable housing, and other community facilities. An EIFD can be used for a single street, in a neighborhood or throughout the entire City. With the WRSP Area being completely undeveloped, there is a unique opportunity to capitalize on the future tax increment value associated with the project build out to help fund significant infrastructure improvements associated with the WRSP Area. The City, working with the developers, will evaluate and initiate the use of an EIFD, as deemed appropriate.

LANDSCAPE AND LIGHTING MAINTENANCE DISTRICTS

Landscaping and lighting maintenance districts (LLMDs) may be used for installation, maintenance, and servicing of landscaping and lighting through annual assessments on benefiting properties. LLMDs may also provide for construction and maintenance of appurtenant features, including curbs, gutters, walls, sidewalks or paving, and irrigation or drainage facilities. They may also be used to fund and maintain parks above normal park standards maintained from General Fund revenues. The City has an existing LLAD and may determine that the WRSP Area join the existing LLAD as a new zone.

Action 2.3: Establish infrastructure phasing based upon development priorities.

This task serves to ensure that a financially feasible and acceptable Financing Plan can be created to support development in the WRSP Area.

8. Establish preferred financing mechanisms.

Implementing the Financing Plan within the context of overall WRSP implementation may require that the City, or State Agency, establish a CFD or assessment district to help improve the developer's ability to meet the City's infrastructure requirements, both with regard to amount and timing of the improvements.

9.5 Maintenance of Common Facilities

Common facilities within the WRSP Area will include both public and private facilities. Determination of public versus private shall be established through the tentative map process. The City shall be responsible for maintenance of public facilities while private facilities shall be maintained under the framework of a Master Homeowner's Association, with sub-associations for individual neighborhoods as deemed appropriate.

9.6 Implementation Policies

9.6.1 ADMINISTRATION

9.6.1.1 DEVELOPMENT AGREEMENTS

To strengthen the public planning process, encourage private participation in comprehensive planning, and reduce the economic risk of development, the Legislature of the State of California adopted Government Code sections 65864 et seq. ("Development Agreement Statute"), which authorizes the City to enter into a development agreement with any person having a legal or equitable interest in real property providing for the development of that property and establishing certain development rights in the property. The City of American Canyon Municipal Code Chapter 19.47 further establishes the authority for the City to enter into development agreements.

The Development Agreement by and between American Canyon 1, LLC (AC-1) and the City of American Canyon (Ordinance 2019-06) establishes the administration of the terms and conditions of the WRSP. The Development Agreement ensures predictable and equitable allocation of

costs of basic and shared infrastructure, reimbursements of improvement costs benefiting other properties, and provisions for reciprocal and mutually benefiting right-of-way dedications and grants of easements for public and private infrastructure. The administration of the terms and conditions of the WRSP for the development of property owned by the Newell Family may be established in separate development agreement(s) between the Newell Family and the City of American Canyon.

The Development Agreement is subject to future modification by a mutually executed document signed by the parties thereto. In the case of a conflict between the WRSP's description of the Development Agreement and the terms and conditions of the Development Agreement, the terms and conditions of the Development Agreement shall control.

9.6.2 ADOPTION

The WRSP was adopted by ordinance on October 16, 2018 (Ordinance 2018-08), in accordance with Section 19.17.070 of the American Canyon Municipal Code.

9.6.3 AMENDMENT

Amendment of the WRSP shall be done in the same manner as adoption of the WRSP. Pursuant to City of American Canyon Municipal Code Section 19.17.070 (D), the WRSP provides provisions for minor and major amendments.

9.6.3.1 WRSP AMENDMENTS - MAJOR

Major WRSP Amendments are those determined by the Community Development Director to be changes that could significantly alter the design intent and vision of the WRSP or affect environmental impacts and approved mitigation measures. Major Amendments to the WRSP must be reviewed and approved by Planning Commission and City Council. The following will likely be considered to be a major WRSP Amendment:

- Introduction of new land use categories.
- Changes in the boundaries of the Land Use-Zoning categories in conjunction with the approval of a tentative subdivision map or parcel map, resulting in an increase or decrease in area of more than 20% or 5 acres, whichever is greater.
- Changes to the circulation system or community facility design which would materially affect a planning concept detailed in this WRSP, or this WRSP as subsequently amended.
- Density increases in excess of the maximum allowable stated density or Density Transfers in excess of limits set in Section 9.8 below.

prior to the expiration of the tentative maps. Multiple final maps may be filed with each phase of development when approved as part of the tentative map entitlement stage.

9.7.1.2 IMPROVEMENT PLANS

Implementation of the WRSP will require the submittal and approval of improvement plans by the City. Improvement plans must include detailed engineered grading, street improvement and utility plans for both on and off site improvements. All improvement plans shall comply with the requirements of the WRSP and other City standards not otherwise specified in the WRSP.

9.7.1.3 DESIGN PERMITS - ARCHITECTURE AND SITE APPROVAL

All Architecture and Site approvals require a Design Permit. Development proposals shall be consistent with the goals, policies and regulations of the WRSP and shall implement the Community Design Guidelines set forth in the WRSP, Appendix A. Architectural and Site Approval shall be required for all development within the Watson Ranch Area and shall be subject to review and approval of the City staff and Planning Commission pursuant to the City of American Canyon Municipal Code Sections 19.41.010 through 19.41.060. Applications shall provide a site development plan, architectural design drawings, landscaping plans and any other information deemed necessary by the American Canyon Community Development Director. All Design Permits may be processed concurrently with Tentative Tract Map application(s).

A single Design Permit may be processed for the NVRG Area. The Design Permit will include all that area defined as NVRG on Figure 4.2 and may include the adjoining open space properties and will include the Parking Management Plan (PMP). This Design Permit may serve as a master plan for the ongoing implementation of the NVRG.

9.7.1.4 CONDITIONAL USE PERMITS

Uses defined as conditional uses under each of the land use categories within the WRSP Area shall be subject to the City of American Canyon Municipal Code Section 19.42.020. An application for conditional use permit shall be submitted to the City of American Canyon for review by City staff and the Planning Commission. The Planning Commission shall review the application at a properly noticed public hearing and consider approval or denial of the Conditional Use Permit pursuant to the required findings set forth in 19.42.020 (D).

9.7.1.5 OTHER APPROVALS

Other approvals as may be required by the City of American Canyon and any other public agency having legal jurisdiction regarding development and use of property within the WRSP (e.g., final map approvals, execution of subdivision improvement agreements, building permits, certificates of occupancy, etc.).

9.7.2 ENVIRONMENTAL REVIEW/CEQA EXEMPTION

In advance of adopting this WRSP, the City of American City Council Certified an Environmental Impact Report (EIR) evaluating the significant environmental impacts associated with buildout of the WRSP and adopted mitigation and monitoring program which has been incorporated by reference into the WRSP.

Under the California Environmental Quality Act (CEQA), the WRSP is a "project" subject to evaluation of potential adverse impacts to the environment. The information obtained in a project's environmental review provides decision makers with the insight necessary to guide policy development, thereby ensuring that the WRSP's policies will address and provide the means to avoid potential environmental impacts.

Although the EIR and the WRSP are separate documents, it is important to note that the environmental review process has been an integral component of the planning process to ensure the WRSP's sensitivity to critical environmental concerns. The WRSP was reviewed as a project anticipated to be developed over an extended period of time. This approach enabled the City of American Canyon to comprehensively evaluate the cumulative impacts of the WRSP and consider alternatives and area-wide mitigation measures in conjunction with the adoption of the WRSP.

Subsection (a) of Government Code Section 65457 states: "Any residential development project, including any subdivision, or any zoning change that is undertaken to implement and is consistent with a WRSP for which an environmental impact report has been certified after January 1, 1980, is exempt from the requirements of Division 13 (commencing with Section 21000) of the Public Resources Code. However, if after adoption of the WRSP, an event as specified in Section 21166 of the Public Resources Code occurs, the exemption provided by this subdivision does not apply unless and until a supplemental environmental impact report for the WRSP is prepared and certified in accordance with the provisions of Division 13 (commencing with Section 21000) of the Public Resources Code. After a supplemental environmental impact report is certified, the exemption specified in this subdivision applies to projects undertaken pursuant to the specific plan".

APPENDIX A: DESIGN GUIDELINES

A.1 Purpose and Intent of the Design Guidelines

The Watson Ranch Specific Plan (WRSP) design guidelines seek to create a unique, memorable identity to reinforce the City of American Canyon as a destination within Napa County.

This design direction utilizes simple forms with an architectural palette appropriate to wine country living. The design guidelines are statements that describe the desired visual character within the WRSP. They are working tools that implement the Vision and Design Strategy provided in Chapter 4. The design guidelines address issues that are primarily aesthetic in nature.

The design guidelines establish a range of encouraged design approaches while allowing for flexibility and innovation. The design guidelines are designed to provide the City of American Canyon with the necessary assurance that the WRSP Area will develop in accordance with the quality and character proposed herein; and to provide guidance to City staff, the Planning Commission, and the City Council in the review of future development projects in the WRSP Area.

The design guidelines address each land use: Napa Valley Ruins & Gardens Mixed Use (MU), High Density Residential (HDR), Medium Density Residential (MDR), Parks, and Open Space; and general categories of community monumentation, as well as walls and fencing. The intent of the design guidelines is to encourage innovative, quality design for all areas of the WRSP Area.

Per City of American Canyon Code, all development proposals must be reviewed by the Planning Commission. Any changes to a building and/or facade for any land use submitted after Planning Commission approval should be reviewed and approved administratively by the planning director or the designee. Should they deem that there has been too great a modification to the plans previously recommended for approval by the Planning Commission and approved by City Council, then the particular building elevations shall be re-submitted for review and approval.

A.2 Sustainability Design Guidelines

The sustainability of a community depends on creating and maintaining its economic and environmental health. It is important that for communities to be successful, they develop principles and a collective vision for the future that applies an integrative approach to environmental, economic, and social goals. The WRSP aims to achieve a high level of sustainability incorporating a three tier approach to sustainability through an effective management of resources at all levels of planning, design, and construction.

TIER 1 DESIGN GUIDELINES

The first tier involves using sustainable principles in community design and overall master planning. An integral relationship exists between how a community is planned and developed through its form, configuration, and use along with its capacity to meet its social, environmental, and economic needs. Community form, which represents the needs and priorities of the community, directly influences community capacity to sustain itself into the future. The sustainability design guidelines used in the overall planning of the WRSP Area include:

- Vehicle miles travelled should be reduced through the provision of a mixed-use community that accommodates a wide range of uses.
- A wide range of housing types should be provided that allow for varied housing opportunities.
- Non-vehicular circulation should be emphasized through the creation of compact, interconnected walkable blocks.
- A healthy lifestyle should be promoted through an integrated system of trails and pedestrian connections.
- Narrow street sections should be provided to reduce surface runoff and reduce urban heat island effect.

- o Protection and preservation of native species and natural vegetation
- o Disease and pest resistance
- o Water-conserving plant and turf species
- o Selection of plants from the East Bay Municipal Water District's "Plants and Landscape for Summer-Dry Climates" as the primary reference.
- Plant materials should be selected that promote a diversity of flora and fauna.
- Recycled water for landscape should be considered to reduce the consumption and demand of potable water.
- Pursuant to Zoning Ordinance Chapter 19.22.040 no turf is permitted in front yards and in other areas turf areas should be minimized.
- Low impact development principles should be implemented to reduce the impact of the development on the natural environment.
- Incorporate infiltration beds, swales, and basins into the design of a project to allow water to collect and soak into the ground, as required by local agency.
- Utilize pervious or porous surfaces (permeable pavers or blocks) to minimize runoff.
- Encourage the use of rain harvesting or catchment technologies (rain gardens, canisters, etc.)
- Projects shall incorporate The City of American Canyon Model Water Efficient Landscape Ordinance (MWELo) into the design.

A.2.1 SUSTAINABLE LANDSCAPE

Sustainable landscape practices proposed for the WRSP Area may include Low Impact Development techniques, innovative irrigation technologies and water conservation, drought-tolerant landscape, and energy conservation.

A.2.1.1 LOW IMPACT DEVELOPMENT (LID) LANDSCAPE

The term LID refers to storm water management techniques that use vegetation and open space to optimize natural hydrologic processes and reduce stormwater runoff.

Areas incorporated within the WRSP Area that provide treatment include NVRG, parks, community gardens, detention basins and open space. All project areas should incorporate the following sustainable landscape practices:

- Large canopy street trees should be used wherever appropriate to intercept rainwater, encourage root uptake, and facilitate evapotranspiration.
- Bio-retention systems in conjunction with vegetated swales should be incorporated where appropriate in open space and other landscape areas.
- Where space allows, residential areas should include landscape treatments that provide on-lot detention, filtering of rainwater, and groundwater recharge.
- Where suitable, permeable surfaces such as unit pavers, turf block, gravel and permeable concrete, should be used in place of paving;
- Tree and plant species for bio-retention and bio-swale areas should be selected from the approved plant palette shown in Appendix A.

A.2.1.2 IRRIGATION AND WATER CONSERVATION

Irrigation systems should be designed to conserve water resources by efficiently and uniformly distributing water. Less watering, fertilizing, and chemical control required for landscape design reduces the need for irrigation and associated water use.

- Irrigation design should follow California Department of Water Resources ordinances, Napa County Water Ordinances and tailored to the climate of American Canyon.
- Irrigation should be provided for plant establishment.
- All public areas, rights-of-way, and community landscaping should have an automatic irrigation system.
- Use of low volume spray heads and drip irrigation systems should be maximized.
- New irrigation techniques and drip irrigation systems should be used for efficient delivery of water.
- Irrigation design should accommodate hydrozones accordingly, separating high, medium and low water-use plants.

A.3.1 OVERALL LANDSCAPE DESIGN GUIDELINES

The following design guidelines pertain to all initial planting within the WRSP Area. As applicable, landscaping within the WRSP Area will be subject to any special requirements identified by soils or inherent drainage conditions.

- Plant materials should be selected from the plant palettes in Appendix A, with an emphasis on native and adaptive plants, where appropriate.
- Substitutions or additions may be considered based on the suitability of the species in terms of similarity of form, adaptability, tolerance to site soils, climatic conditions or water quality, or other pertinent characteristics.
- The plant list is not intended to be exhaustive but to provide a clear guide for selection.
- Additional plants may be used that are compatible with this list and are consistent with the intent of these Guidelines.
- Plant selection should emphasize the use of drought-tolerant, long-lived plant species that are well adapted to the climatic and soils conditions of the site.
- Planting design should consider year-round interest and seasonal character through the careful use of flower, leaf color and texture.
- Landscape design should provide effective screening of parking areas, retaining walls, utility enclosures, utility cabinets, service areas, or transportation corridors to reduce negative visual impacts.
- Screening landscape should incorporate evergreen plant species in order to maintain year-round leaf cover.
- Plant selection should avoid the use of tree species with invasive root systems near utility lines and paving and avoid the use of non-native, invasive species that may spread into open space areas.
- Street trees should be placed in parkway strips between the curb and sidewalk, with drought tolerant shrub and ground cover plantings to encourage sidewalk use.
- Pedestrian trails or walkways with ornamental plantings may provide enhanced landscape within the various neighborhood communities.
- Landscaping is required where development is visible from major public roadways and public facilities including trails.
- Tree planting location and species should consider the need to preserve solar access, views, and fire safety requirements.
- All plants should be carefully selected to avoid toxic species that could be harmful to children or cause allergic reactions.
- Landscape plans for all areas where the developer is required to install landscaping should be prepared by a landscape architect registered to practice in the State of California.



Edible Landscape



Community Garden

in the use of ornamentation. Materials such as smooth stucco, wood, and metal roofing should be used. Detailing should be of wood and metal. A mix of rich but muted colors with bold accents can provide vibrancy to a streetscape. Form-based architecture should result in simple structures where gable and shed roofs with tower elements should be used as accents and focal points.

A.4.2.1 CREATE VARIETY ALONG THE STREETScape

Creating building relationships and massing that demonstrate variety and individuality will add to the sense of place.

- There may be a mix of two and three story homes within the community.
- Unvarying repetitive facades that present a monolithic development should be avoided.

- Where practical, windows should be placed to minimize privacy conflicts.
- Visual interest in the streetscape should be created through building articulation and a variety of forms between buildings.
- There should be varied wall plane lines.
- There should be publicly visible balconies, veranda, porches, and arcades.
- Front porches should be sufficiently sized to be usable.
- There should not be repetitive, unarticulated building forms.
- Unarticulated roof forms should not be set on a constant wall plate height.



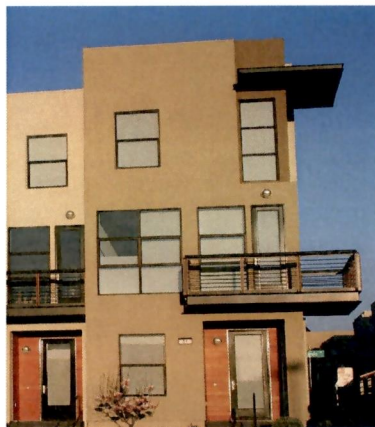
Appropriate use of accent material to create a signature architecture style



Stucco finish with metal roof



Appropriate change of materials to accent design intent



Smooth stucco finish with accent material

A.4.3.1 WALLS AND FENCING

Fences and walls should be built according to the design criteria and be of the materials indicated in these Design Guidelines. Retaining walls over eighteen (18) inches in height should be of masonry or poured in place concrete construction, and any retaining wall over three (3) feet shall not be installed without structural calculations developed by a structural engineer. All wall locations, heights, and finishes shall require approval of the City of American Canyon.

Fencing and walls at property lines, and between neighborhoods and adjacent to land uses are highly visible and are a major component of the visual landscape. The goal of fencing and walls in the WRSP Area is to maintain an open park setting that unites and blends private landscape with the lake, parks, open spaces and urban areas while providing a sense of privacy and security to homeowners with sound attenuation.

A.4.3.2 RESIDENTIAL LANDSCAPE GRADING AND DRAINAGE

All site surface drainage from private landscapes should be conducted to a storm water quality treatment system by the Builder.

Mounding and earth berms are discouraged as part of Private Landscape design.



Variety of shrubbery

A.4.3.3 RESIDENTIAL LANDSCAPE IRRIGATION

The WRSP is committed to water conservation and the efficient use of water through innovative and accepted irrigation practices. Irrigation designers shall use current water use guidelines and tables, state-of-the-art irrigation equipment, and automatic controllers capable of multiple programming.

- Drip irrigation is encouraged particularly in small planting areas.
- Overhead irrigation systems may be used in larger planting areas.
- Overspray onto paving, fences or walls should be avoided and soil erosion should be minimized by the use of in-line or in-head check valves.
- Turf areas should be irrigated by an overhead spray or rotor system or by subsurface irrigation drip tubing. Overspray onto paving, fences or walls should be avoided and soil erosion should be minimized by the use of in-line or in-head check valves.
- All valves and equipment should be located adjacent to buildings where feasible and visually screened from public view.
- No irrigation equipment should be located in such a way so as to create a safety hazard to persons or property.
- Operating manual and scheduling charts should be provided to all Homeowners, Maintenance Companies, Associations, or Agencies.



Appropriate landscaping adjacent to sidewalks

PLANT COMMUNITY

BOTANICAL NAME	COMMON NAME	PLANT COMMUNITY						EDIBLE	LOW WATER USE
		RESIDENTIAL NEIGHBORHOOD	NEIGHBORHOOD PARKS	PARKS & OPEN SPACE	OPEN SPACE BUFFERS	QUARRY LAKE PARK	NAPA VALEY RUINS & GARDENS		
TREES									
Arbutus 'Marina'	NCN								
Arbutus unedo	Strawberry Tree								
Betula pendula	European White Birch								
Camellia	Camellia								
Carpinus fastigiata	European Hornbeam								
Cedrus deodara	Deodar Cedar								
Cercis canadensis	Eastern Redbud								
Citrus sp.	Orange/Kumquat								
Cupressus sempervirens	Italian Cypress								
Eriobotrya deflexa	Bronze Loquat								
Ginkgo biloba	Maiden Hair Tree								
Lagerstroemia faurei	Crape Myrtle								
Laurus nobilis 'saratoga'	Grecian Laurel								
Liquidambar styraciflua 'festival'	American Sweetgum								
Magnolia 'St. Mary's'	Magnolia								
Malus sp.	Apple								
Maytenus boaria	Mayten Tree								
Melaleuca sp.	Paper Leaf Tree								
Melaleuca quinquenervia	NCN								
Olea europaea	Olive								
Palm trees	Palms								
Pistacia chinensis	Chinese Pistache								
Pinus eldarica	Japanese Black Pine								
Pinus pinea	Italian Stone Pine								
Platanus acerifolia 'Columbia'	London Plane Tree								
Populus nigra 'Italica'	Lombardy Poplar								
Prunus sp.	Cherry/Plum								
Prunus sargentii 'columnaris'	Sargent's Cherry								
Punica granatum	Pomegranate								
Pyrus calleryana 'Chanticleer'	Flowering Pear								
Pyrus calleryana 'Bradford'	Flowering Pear								
Quercus agrifolia	Coast Live Oak								
Quercus lobata	Valley Oak								
Quercus virginiana	Southern Live Oak								
Tilia cordata	Little Leaf Tilden								
Ulmus parvifolia 'True green'	Chinese Elm								

GROUNDCOVER													
Abelia Prostrate Form	NCN												
Arctostaphylos spp.	Prostrate Manzanita	Yellow	Light Green	Dark Green	Light Blue	Light Purple	Red						Brown
Ceanothus spp.	Prostrate Wild Lilac		Light Green		Light Blue								Brown
Coprosma spp.	Coprosma	Yellow											
Cotoneaster spp.	Cotoneaster		Light Green	Dark Green		Light Purple	Red						Brown
Fragaria chiloensis	Wild Strawberry	Yellow	Light Green	Dark Green									
Fragaria x ananassa	Garden Strawberry		Light Green	Dark Green	Light Blue	Light Purple	Red		Yellow				Brown
Juniperus spp..	Juniper			Dark Green									Brown
Lantana montevidensis	Lantana	Yellow	Light Green			Light Purple	Red						Brown
Myoporum parvifolium	NCN		Light Green	Dark Green		Light Purple							Brown
Rosa spp.	Groundcover Rose												
Rosmarinus spp.	Rosemary				Light Blue							Yellow	Brown
Salvia spp.	Sage				Light Blue							Yellow	Brown
Tuecium spp.	Germander						Red						Brown
Thymus spp.	Thyme		Light Green	Dark Green			Red					Yellow	Brown
Trachelospermum asiaticum	Jasmine	Yellow											
Verbena spp.	NCN	Yellow	Light Green				Red						
GRASSES													
Acorus spp.	Forest Grass	Yellow											
Carex spp.	Sedge	Yellow	Light Green	Dark Green			Red						
Eleocharis spp.	Spike Rush	Yellow	Light Green	Dark Green									
Festuca spp.	Fescue	Yellow	Light Green	Dark Green	Light Blue		Red						
Juncus spp.	Rush	Yellow	Light Green	Dark Green									Brown
Leymus spp.	Wild Rye	Yellow	Light Green	Dark Green		Light Purple	Red						Brown
Miscanthus spp.	Eulalia Grass	Yellow	Light Green	Dark Green	Light Blue	Light Purple	Red						Brown
Muhlenbergia capillaris	Hairy Awn			Dark Green	Light Blue	Light Purple	Red						Brown
Pennisetum spp.	Fountain Grass	Yellow	Light Green	Dark Green	Light Blue	Light Purple	Red						
TURF													
Fescue Turf	Fescue	Yellow	Light Green	Dark Green	Light Blue	Light Purple	Red						
Festuca rubra	No-Mow Fescue	Yellow	Light Green	Dark Green	Light Blue	Light Purple	Red						

- No direct view to light sources should be visible from off site.
- Where lighting is provided, fixtures should utilize house side shields and cut-off optics to reduce light spill over the property line.
- Pedestrian pole light fixture locations should not conflict with the pattern of tree planting along the roads and parking lots.
- Pole-mounted lights should be used sparingly in the Civic Park Plaza and the use of illumination encouraged for pedestrian circulation and less ambient light at night.
- Illuminated bollards should be used primarily along pedestrian pathways to encourage evening strolls.
- Accent lighting should also be used sparingly and primarily for the use of highlighting nodes or destination spaces.

A.4.6 RESIDENTIAL MAILBOX DESIGN GUIDELINES

- Mailbox materials should be consistent with architectural theme.
- Multibox stations should be designed to replicate architectural details and colors.

A.5 Napa Valley Ruins & Gardens Design Guidelines

The mixed-use NVRG is a unique area made up of several different uses including retail, residential, office, commercial, visitor service including hotel and events center, entertainment, recreation, civic, and community facilities. This blending of uses creates an urban fabric with distinct, yet connected areas.

As the “heart” of the WRSP Area, the NVRG creates the hub of activity for the overall community. The quality of design is important to the perception of the Watson Ranch community. Architecture and landscaping should create a village atmosphere where people are comfortable to mix and mingle and desire to return to over and over again.

A.5.1 COMMUNITY CHARACTER DESIGN GUIDELINES

The NVRG will create a strong focal point for the new community. While having the flexibility to be the setting for community gatherings and special events, the area must also provide for comfortable, more intimate resting spaces. This is a place where all residents and visitors should feel comfortable whether they are a participant in an activity such as an art fair or street performance, or quietly sipping coffee, spending time with family, shopping, people watching or playing checkers.

A.5.1.1 THE PEDESTRIAN ENVIRONMENT

NVRG should create an environment where people are comfortable walking and spending time, and should incorporate the following Design Guidelines:



Linear outdoor seating, along sidewalks



Enclosed outdoor seating, in a courtyard

ruins as well as from the vineyard heritage of Napa County. Even though the ruins are over 100 years old, their simple shapes form a very contemporary backdrop for the community.

A.5.2.1 BUILDING ENTRANCES

- Buildings should be oriented to face streets or other public spaces.
- Entrances should be well marked, articulated, and oriented to streets and open space.
- Entrances should provide opportunities to create unique addresses along the street through use of elements such as distinctive form, detail, materials, color, ornament, lighting and signage.
- Street addresses should be clearly displayed and must comply with applicable sign regulations (see Section 6.5 for signage regulations).
- Entrances to uses above the ground floor should be identifiable as building entrances.
- The design of a secondary side or rear entry should be architecturally related to the front entry and enhanced with detailing, trim, and finish consistent with the character of the building.

A.5.2.2 MASSING AND FORM

- Roofs should match the building in terms of style, detailing and materials and should contribute expressive and interesting forms that add to the overall character of its environment.

- The location, spacing, materials, and colors of downspouts, gutters, scuppers, and other roof drainage components should be incorporated into the architectural composition of the façade and roof. Downspouts should be concealed within walls or located to harmonize with window spacing and façade composition.
- Any mechanical penthouses and stair towers should appear as integrated building forms and shall be structures that complement the design of the building through the use of similar materials, colors, finishes and architectural details.
- Rooftop equipment should be located away from the street edge and/or screened so that it is not visible from streets or other public spaces. Screening should complement the design of the building through the use of similar materials, colors, finishes and architectural details and appears to be an integral part of the building's form.
- Attached equipment such as solar collector panels, antennas, satellite dishes, etc. should be integrated into the project architecture or screened from view.

The main area of the ruins creates a series of cloisters. New buildings within the cloisters of the ruins should have a contemporary aesthetic.

- Any building within the southern cloister should have a prominent roof shape that pays homage to the roof structure on the round building. This bold form would create a focal point within the ruins complex.



Use of trellis / awning to weather protection and to lend a pedestrian scale to street level



Long walls punctuated with 3 dimensional details

- Architectural enclosures should be designed as integral elements of the building architecture
- Long unbroken forms and flat planes are discouraged.

Buildings facades facing public spaces should incorporate 3-dimensional facade elements that lend a pedestrian scale to the street level.

- Trellises or permanent awnings should be incorporated where appropriate for shade and weather protection.
- Arcades, wide overhangs, deep reveals, permanent awnings, etc. should be used.

Roof design should be integral to the overall building design.

- Parapets should screen rooftop mechanical equipment from ground level view.
- Tower elements should be included at key locations to provide points of interest along the streetscape.
- Roofline variation should be created by differing plate heights.

A.5.2.4 BUILDING MATERIALS

Building materials should be appropriate to the community character and suited to commercial construction.

- Materials such as stucco, masonry, storefront glazing and well-detailed precast concrete should be used.
- Accent materials such as brick, stone, tile, and anodized or painted metals should be used.



Materials reflect the industrial history of the site



Facades facing public spaces with articulation



Color and material relate to the context

- Olive trees should be planted in rows symbolic of orchards in Italy and the Napa Valley.
- Lavenders, roses, and evergreen groundcovers should be primarily used to enhance entries, highlight sitting areas, and line walkways.

A.5.3.2 NVRG STREET FURNITURE/ SEATING DESIGN GUIDELINES

Street furniture will add to the unique architectural environment of the NVRG.

- Street furniture should be designed to foster comfortable, accessible, interactive public areas.
- Street furniture should be constructed of durable non-weathering materials utilizing recycled or eco-friendly materials where practical.
- Seating should be provided with a variety of furnishings such as benches, chairs, and low walls with landscape features such as salvaged plinths.
- Other street furniture should include waste and recycling receptacles, decorative planters and pottery, bicycle racks, flag poles, and drinking fountains.

A.5.3.3 NVRG PARKING LOT LANDSCAPING DESIGN GUIDELINES

- Landscaping for parking areas should include large dome-shaped canopy trees to provide shade in hot summer months while allowing winter sun exposure.
- Parking lot trees should have root barriers.

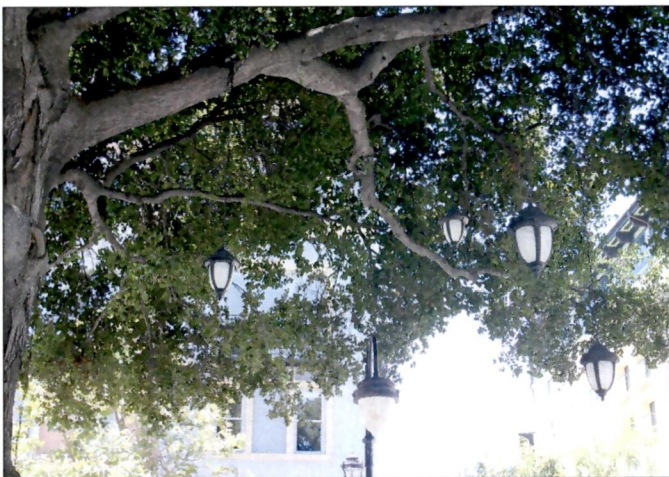
- Understory planting should be primarily with low-growing (+24"), drought tolerant, evergreen groundcover or grasses.

A.5.4 NVRG LIGHTING DESIGN GUIDELINES

These lighting design guidelines provide criteria that will provide a comfortable level of illumination that meets the community's needs for orientation and safety in a way that complements the aesthetic qualities of the architecture and surrounding environment. A family of light fixtures is selected for the development that recognize the different qualities of the land uses while creating a consistent design theme throughout the community. The quality of the light fixtures, the lighting intensity and source controls provide criteria that will establish a high quality experience to this important aspect of design.

A.5.4.1 LIGHT FIXTURE DESIGN GUIDELINES

- High efficiency fixtures and sophisticated optics are encouraged to direct light where it is needed without creating excessive glare.
- Long lasting high pressure sodium lamps are suggested to minimize energy use and lamp replacement.
- Lights should be placed where they are needed for specific uses, rather than a continuous foot-candle requirement across the site, allowing for the appreciation of the dark sky in the residential neighborhoods.
- To preserve the quality of a dark sky at night, high intensity light fixtures should include a shielded light source that reduces the view to the light source.



Lighting integrated to the landscape to highlight gathering spaces



Unique signage

All signage within the WRSP Area shall conform to the specific requirements, spirit, and intent of the City of American Canyon code provisions regulating signage, unless otherwise stated. If there are any inconsistencies between the City of American Canyon regulations and the WRSP, the guidelines in the WRSP will take precedence.

5.5.1 GENERAL SIGN DESIGN GUIDELINES

The building architecture should be designed to accommodate signage and other graphics as an integral part of the building design.

- Illuminated signs should be fixed and directed to control and avoid light pollution
- Individual letter characters are encouraged.
- Metal signs may be made of aluminum, brass, bronze, copper, or stainless or welded steel.
- Raised “Halo” letters on building face; pedestrian oriented blade signs; sculptured cantilevered signs; non-internally lit signs with lighting from a secondary source; and artistic neon signs backed by building face or storefront are encouraged.
- Signage is intended to be unique and original for each establishment and executed with a high degree of craftsmanship.



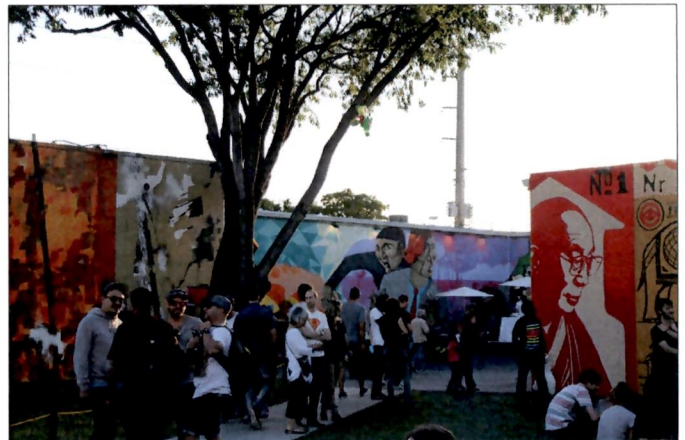
Community art

A.6 Community Art and Monumentation

A.6.1 COMMUNITY ARTS PROGRAM

The WRSP strongly supports the inclusion of community art as an enrichment of the public community space. Art blended with urban design in the NVRG can add value to the experience of the place. Community art should not only be an individually commissioned pieces, but an attitude towards the elements that make up the public realm as experienced within a community. Given the unique identity established by the NVRG, community art can have a complementary role in energizing and defining the spaces. The NVRG may include items such as:

- A thoughtful and carefully selected lighting program that uses fixtures to establish a character.
- Adding banners and hanging planters or other decorative items to lighting fixtures.



Art in community gathering spaces



Infrastructure as art

A.6.2 COMMUNITY MONUMENTATION

A cohesive program of community identification; form the community threads which tie all aspects of the community together within the context of varied architecture.

Community monumentation is a strong visual element that defines and enhances a community's special sense of place. These elements define a district within the greater context of a town. The community's personality is displayed by these features. The icons of classic communities are traditionally monuments that signalize your arrival and an entry or focal point.

- The palate of identification for the WRSP Area is based upon the free standing monuments that exist as part of the ruins.
- The monumentation may include pilasters, low walls, or strategically placed pieces of community art.
- They may also be a focal point created as an element of a building such as a tower, or a publicly visible landscape or retaining wall.

The design of the community identification is unique and takes its cues from the ruins. Materials have a permanence to them, complement the architecture of the community or create a counterpoint, and may include materials such as steel or other metals, concrete, tile, brick, stone and glass. A typical pilaster monument is shown here.

Monumentation locations are identified on the following plan:

A.7 Walls and Fencing Design Guidelines

A.7.1 GENERAL DESIGN GUIDELINES FOR WALLS

- Walls and fences are to be minimized to the greatest extent possible and their design should be complementary to the building architecture.
- Site walls should be made of materials complementary to the building architecture palette.
- Walls, which are located in visually prominent locations, should be decorative and faced with a material such as stucco or stone.
- Concrete retaining walls should only be used where necessary for structural and spacial requirements.

- Plantings should be utilized to minimize the visual impact of all retaining walls.
- Walls, fencing materials and colors should complement adjacent architecture. Typical fence designs are shown here.

A.7.2 GENERAL DESIGN GUIDELINES FOR FENCES, SOUNDWALLS AND VIEW FENCES

- Fences throughout residential landscapes should be constructed of wood, metal, masonry, or a combination of these products to maintain the community theme and provide continuity.
- There are four (4) distinct levels of fencing and walls, such as standard privacy fencing, enhanced privacy fencing, enhanced soundwalls, and view fences.
- Standard six-foot (6') high privacy fencing of a 'good neighbor' type should be used on all fences located between private lots which are not visible to the public view.



Combination of Fencing Materials



Traditional residential wood fence

APPENDIX B GENERAL PLAN CONSISTENCY

B.0 General Plan Policy

A “town center” should be established in proximity to the closed basalt plant which functions as the symbolic center of American Canyon. A wide diversity of uses characterized by a high level of activity should be accommodated, including government, retail, office, service, entertainment, housing, and open space. The center should be developed as a pedestrian-oriented village that is physically linked by pedestrian and bicycle trails and other elements to surrounding neighborhoods and districts.

Watson Ranch Specific Plan (WRSP) calls for housing, parks and open space, and a non-residential component – the Town Center Mixed Use area (TC-1 MU) also referred to as the Napa Valley Ruins & Gardens (NVRG), which allows retail, entertainment, hotels, events center for weddings, receptions, farmers’ market, restaurants, winery, wine tasting and related uses. The WRSP also provides a site which would allow public and or private community facilities such as church, day care, non-profit community-oriented service or government offices.

The WRSP will function as the symbolic center of the city due to the following aspects:

- Retention of iconic structures such as the rotunda building.
- High level of activity programmed for the TC-1 MU area, including both public and private functions.
- WRSP Circulation Plan includes multi-use trails which link TC-1 MU subarea to the remainder of the WRSP, existing residential neighborhoods, and other off-site destinations.

B.1 Land Use Element

1.6.7 Work with local agricultural producers to consider the possible establishment of farmers markets, festivals, site tours, and other events that emphasize the agricultural and rural character of the region. (I 1.23)

1.11.8 Require that any lands proposed for dedication to the City (parks, schools, etc.) be usable and appropriate for the intended use and a source of funds to maintain the area be confirmed to ensure that inappropriate costs are not shifted to the City. (I 1.1, I 1.2, I 1.4, I 1.5, and I 1.8)

Events and festivals open to the public such as a farmers’ market and community gardens are allowable uses for the TC-1 MU area.

Chapter 9 (Implementation) proposes the establishment of a Community Facilities District (CFD) and/or a Lighting Landscaping Assessment District (LLAD) to pay for the cost of public park maintenance, street lighting and other similar improvements. There will be Homeowners Association (HOA) for the residential portion of the WRSP. The HOA will maintain any private recreational facilities and common areas. See Implementation Chapter for financing information.

1.12.1 Accommodate the development of parks, schools, libraries, community meeting facilities, religious facilities, and similar community-serving uses in all residential areas, provided that they are compatible with the intended residential function and subject to City review and approval. (I 1.1, I 1.2, I 1.4, I 1.5, and I 1.8)

1.15.4 Require that mixed use structures and sites be designed to mitigate potential conflicts between the commercial and residential uses, considering such issues as noise, lighting, security, and truck and automobile access. (I 1.1, I 1.4, I 1.5, 1.11, and 1.14)

1.15.5 Require that mixed-use developments be designed to provide adequate transitions with adjacent land uses, which may include horizontal and vertical setbacks, landscape, screening elements, and similar techniques. (I 1.1, I 1.4, I 1.5, 1.11, and 1.14)

1.19.6 Provide for the extension of Newell Drive, which will define the eastern boundary of the Town Center, connecting with Highway 29 in the vicinity of Green Island Road and for the extension of South Napa Junction Road from Highway 29 to Newell Drive.

1.19.7 Determine the location of land uses within the Town Center through the subsequent approval of one or more Specific Plans.

1.19.8 Determine the range of residential densities allowed through the subsequent approval of one or more Specific Plans, with the guiding principle that, in general, densities will be highest around the Town Center Core Area with decreasing density further away

1.19.9 Determine the range of intensities of non-residential uses, as measured in building height and/ or floor area ratio, through the subsequent approval of one or more Specific Plans, with the guiding principle that in general, intensity will be highest around the Town Center Core Area with decreasing intensity further away.

1.19.10 Provide for unified design standards and a cohesive development through the adoption of one or more Specific Plans for the Town Center

The WRSP provides for approximately 33 acres of public parks . The acreage of the public parks exceeds the City's park land dedication policy of 5 acres of public park land per 1000 residents.

The WRSP provides an alignment for Newell Drive along the eastern boundary of the property. The alignment is consistent with the Circulation Element depiction. This alignment is within the boundary of the WRSP and within the existing city limits.

The WRSP determines the location of land uses within the entire area designated by the City's General Plan as "Town Center".

The WRSP provides for three residential density categories: medium density (MDR-12 land use designation – between 2 and 12 dwelling units per gross developable area), medium density (MDR-16 – between 8 and 16 dwelling units per gross developable acre), and high density (HDR – minimum of 20 dwelling units per gross developable acre).

The Land Use Plan shows that MDR-16 and HDR areas are immediately around or within the NVRG. The medium density (MDR-12) residential is generally further from the NVRG Area.

Chapter 4 contains a description of the range of non-residential uses within the WRSP. Appendix A contains design guidelines for WRSP.

1.19.11 Ensure that the Town Center is a sustainable, "green" development through the implementation of low impact development practices, highlighted in Appendix A. Such features could include:

- a. use of recycled water for landscape irrigation
- b. use of drought tolerant vegetation
- c. energy efficient buildings
- d. pedestrian and bicycle circulation system
- e. mix of land uses which reduce travel

1.19.12 Require that the Town Center Core Area provide a pedestrian-oriented, "village environment, including a plaza as a gathering place for community activities.

1.19.13 Require the implementation of public streetscape improvements that uniquely identify the Town Center, including elements such as landscape, street furniture, signage, and lighting; public street sections may vary from citywide standards in order to create this unique identity.

1.19.14 Require that development of Town Center incorporate the natural and cultural resources on site including:

- a. preserving portions of the basalt industrial ruins and incorporating them into the commercial and public activities, to the extent it is economically viable;
- b. preserving the quarry lake as a future public park site, for the benefit of both Town Center residents and the residents of the City as a whole.

The WRSP details out low impact development practices, outlined in Appendix A. The WRSP requires use of recycled water for landscape irrigation for major park and arterial landscape parkways, and provides guidance for the use of drought tolerant landscaping, and energy efficient homes. The Plan includes an extensive system of neighborhood multi-use paths (pedestrian and bicycle) which links neighborhoods with parks and

B.2 Housing Element

PROGRAM 2.1.2

Sustain residential land use designations that:

1. *Include a minimum density for each designation's range of permitted densities.*
2. *Allow flexibility in the types of units that may be constructed in master-planned communities and other planned developments.*
3. *Include a density category that, when combined with an affordable housing density bonus, is sufficiently high enough to facilitate the development of lower-income housing.*

The residential portion of WRSP provides three density categories: LDR, MDR, and HDR. Each density category has a minimum and maximum density as defined by the City's General Plan.

WRSP will be a master planned community. It provides flexibility in the types of housing units as follows:

4. *Medium Density Residential allows single family detached, single family attached and townhouses*
5. *High Density Residential allows both apartments and townhomes*
6. *The Implementation Chapter 9 allows a density transfer, which means the redistribution of residential units from one planning area to another, under specified procedures and criteria*

The High Density Residential has a density range of 18 to 30 dwelling units per acre

Policy 2.2.1 Allow flexibility in the type of units developed on vacant, residentially designated properties in master-planned communities and other planned developments.

Policy 2.2.2 Require larger projects to include a mix of housing types

Policy 2.2.3 Encourage the development of residential uses in association with compatible nonresidential uses.

Objective 2.3 Promote residential design that is functional, people and pedestrian-oriented, aesthetically pleasing, and contributes to a sense of community through the sensitive arrangement of buildings, open space (public and private), and circulation (vehicular and pedestrian). Encourage innovative and creative design in residential projects.

Policy 2.3.1 Ensure that new residential development fulfills the above objective through the establishment and application of comprehensive design guidelines and development standards.

Objective 2.6 Expand the availability of affordable housing in American Canyon

WRSP will be a master planned community. It provides flexibility in the types of housing units as follows: Low Density Residential allows both single family detached and secondary living units

Medium Density Residential allows single family detached, single family attached and townhouses High Density Residential allows both apartments and townhomes

The Implementation Chapter 9 allows a density transfer, which means the redistribution of residential units from one planning area to another, under specified procedures and criteria.

WRSP allows a broad range of housing types including single family attached, single family detached, townhomes, apartments, condominiums, and secondary living units.

WRSP proposed residential uses in conjunction with nonresidential uses.

The WRSP Circulation Plan provides an extensive system of multi-use trails linking the plan area and providing connections to regional trails. WRSP provides approximately 36 acres of public and private open space.

WRSP contains comprehensive design guidelines and development standards.

WRSP does not call for the construction affordable housing within the project boundaries.

PROGRAM 2.13.1 Require all residential projects of ten or more above moderate-income units to include affordable units.

The intent of the Inclusionary Alternatives is to provide options for developers while still meeting the City's affordable housing demand. Alternatives to providing on-site inclusionary units are 1) the provision of the units at an acceptable off-site location or 2) payment of in-lieu fees to the City's Housing Fund, to be used in the provision of housing affordable to lower-income households, or 3) the dedication of suitable land to the City for future inclusionary units, or 4) if the project exceeds its required inclusionary amount, the project owner may request inclusionary credits be counted toward another project subject to the approval of the City Manager

1.24 Impacts of new development. Based upon the findings of a transportation impact analysis, consistent with Guiding Policy 1.26, new development will be responsible for mitigation of transportation related impacts.

1.34 Rights-of-Way fully within master plan boundaries. Planning areas shall not use roadway centerlines as boundaries. Roadways shall be built to their full width within the annexed City limits. Part-width roads shall not be permitted where master plan areas abut unincorporated properties that are not expected to be annexed to the City within the time frame of this General Plan. Road rights-of-way that demarcate the edge of a planning area shall be fully contained within the development area boundary, and expanded only within that boundary.

4.10 New railroad crossings. Provide new crossings across the railroad (UPRR) in conjunction with the planned roadway improvements shown on Figure 3. The provision of new crossings will ensure at least one cross-town route is maintained in case rail activity interrupts local and regional traffic flow and/or emergency access. New crossings will be at: Newell Drive (grade separated); and Rio Del Mar or South Napa Junction Road (initially at-grade, may be grade separated in the future).

An Environmental Impact Report was prepared by the City prior to its consideration of WRSP. A determination of the LOS was made through the EIR process.

New development shall mitigate its share of transportation-related impacts.

The alignment for Newell Drive extension is proposed along the eastern boundary of the WRSP. The proposed roadway is entirely within the city limits and entirely within the boundary of the WRSP.

The WRSP incorporates a below grade or at grade crossing at Rio Del Mar connecting Highway 29 with Newell Drive. This will ensure a cross town connection without interruption by the rail activity.

Should an at-grade or grade-separated railroad crossing at Rio Del Mar prove unfeasible, an alternate route for providing the connection with Highway 29 is South Napa Junction Road. There exists a private at-grade railroad crossing at South Napa Junction that is the subject of a previous settlement agreement.

B.5 Utilities Element

5.8.1 Require improvements to the existing water supply, distribution, storage, and treatment facilities necessitated by a new development proposal be borne by the project proponent (in proportion to benefit); either through the payment of fees, or by the actual construction of the improvements. (I 5.2, I 5.8, and I 5.9)

5.10.3 Require that adequate storm drain and flood control facilities be constructed coincident with new development. (I 5, 24, 15,25, and I 5.33)

5.11. Require improvements to existing storm drain and flood control facilities necessitated by a new development proposal be borne by the project proponent; either through the payment of fees, or by the actual construction of the improvements in accordance with State Nexus Legislation. (I 5.30, I 5.31, and I 5.33)

5.16.1 Require that the cost for improvements to the existing wastewater collection and treatment facilities necessitated by a new development proposal be borne by the project proponent in proportion to benefit; either through the payment of fees, or by the actual construction of the improvements. (I 5.51 and I 5.52)

The Infrastructure Chapter of WRSP identifies the improvements for water distribution, storage and treatment necessary to serve the proposed development. A Water Supply Assessment was prepared in conjunction with the EIR. New development with WRSP will make a proportionate contribution to the City's water distribution, storage and treatment facilities.

The Infrastructure Chapter of WRSP identifies the storm water detention and conveyance facilities needed to serve the development. The WRSP states that appropriate storm water detention and conveyance facilities shall be constructed with each phase of development. These facilities will be designed and constructed to accommodate a 100-year storm event.

New development with WRSP will make a proportionate contribution to the City's wastewater collection and treatment facilities.

The Development Standards require multifamily residential development to include private open space at a ratio of 80 square feet per dwelling unit and 150 square feet of common open space per dwelling unit. The common open space would provide for recreational and open space facilities onsite.

7.7.4 Design and improve community and neighborhood parks according to the following:

- a. locate on collector or neighborhood streets, accessible to adjacent residential neighborhoods;
- b. site uses so that they do not adversely impact adjacent residences (e.g. locate high activity, noise-generating uses away from residences);
- c. provide parking so that it does not disrupt abutting residences;
- d. design for defensible space;
- e. site parks away from high noise generators (highways) and other nuisances (i.e., power lines); and
- f. incorporate park landscape that is compatible with the landscape of adjacent areas. (I 7.4, 1 7.5, and 1 7.28)

As shown on the Land Use Plan, all public parks are located on collector or neighborhood streets and accessible to surrounding residential neighborhoods.

Landscaping within the parks should support the overall character of the Watson Ranch Community as defined by the Design Guidelines.

B.8 Natural & Historic/Cultural Element

Goal 8E To promote the preservation and restoration of the sites, structures and districts that have architectural, historical, archaeological and/or cultural significance to the City of American Canyon.

Objective 8.19 Ensure that the City's historically and archaeologically significant resources are protected in a manner that preserves and/or enhances the resources' inherent historic value,

Policies

8.19.1 Conduct a comprehensive survey of archaeological and cultural resources and historic vegetation that is based on established criteria and encompasses the entire City and its Sphere of Influence. (I 8.40)

8.19.2 Adopt a Preservation Ordinance that will authorize the City to designate appropriate vegetation or archaeological sites deemed to be of historic, archaeological, or cultural significance an American Canyon City Historic Point, Site or District. Such an ordinance shall conform to state and federal criteria for establishing a preservation ordinance. (I 8.5, I 8.48, and I 8.49)

8.19.4 Though the design review process, encourage compatibility between new development and existing adjacent historic structures in terms of scale, massing, building materials and general architectural treatment. (J 8.29)

8.20.4 Prohibit demolitions if other alternatives exist that enable a property owner to sensitively add to the existing structure, or develop an accompanying building on the site that allows property development rights to be realized. Variances of setbacks, heights and parking requirements should be given to make the preservation of an existing historic building feasible when no other reasonable alternative exists. (J 8.49)

8.20.4 Prohibit demolitions if other alternatives exist that enable a property owner to sensitively add to the existing structure, or develop an accompanying building on the site that allows property development rights to be realized. Variances of setbacks, heights and parking requirements should be given to make the preservation of an existing historic building feasible when no other reasonable alternative exists. (J 8.49)

8.20.5 Encourage appropriate adaptive reuse of historic resources such as the Basalt Plant in order to prevent misuse, disrepair and demolition, taking care to protect surrounding neighborhoods and/or agricultural land from incompatible uses. (I 8.49)

The City has not established criteria or conducted an archaeological and cultural resources survey for the city and its Sphere of Influence. An archaeological survey was prepared by ESA (dated 4/2006). Further assessment was included in the EIR. The evaluation of the resources onsite will be evaluated in accordance with applicable State and federal criteria and guidelines.

The City has not adopted a Preservation Ordinance.

8.13.1 Encourage the preservation of agricultural uses on the City's periphery through the creation of a permanent greenbelt. (I 8.11, I 8.17, r 8.26, I 8.28, and I 8.41)

Through the initiative process, the City has adopted an Urban Limit Line ("ULL"), as part of its General Plan. In